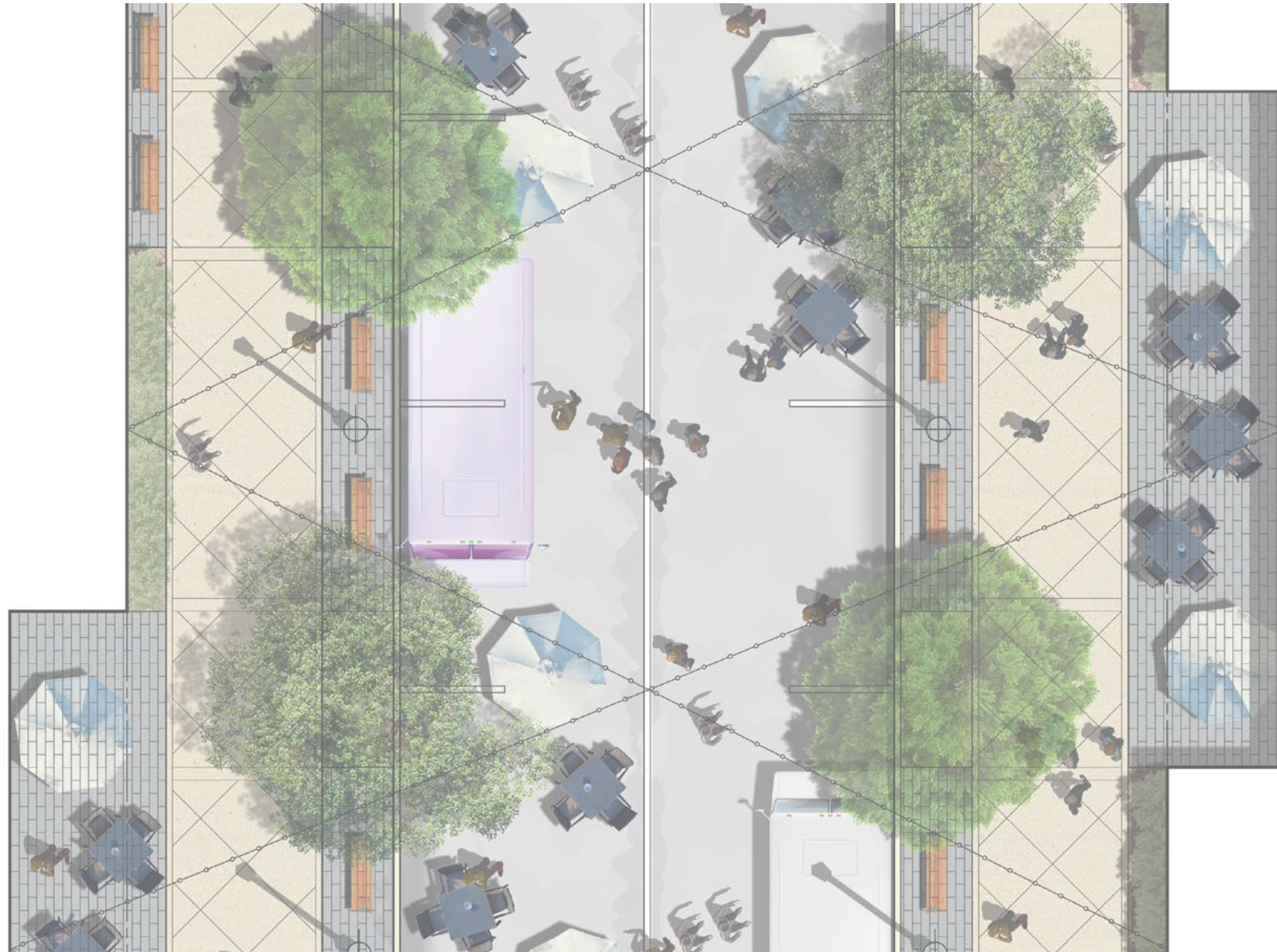


Diamond District
Public Realm | Design Standards

*City of Richmond, Virginia
July 10, 2024
Vanasse Hangen Brustlin, Inc.*







ACKNOWLEDGEMENTS

As we present these design standards, we wish to extend our deepest gratitude to all those who have contributed to its completion. We thank The City of Richmond for their time and effort in the development of this report.



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1 | EXECUTIVE SUMMARY

The Diamond District Public Realm Design Standards is a vital tool for the redevelopment of the Diamond District. The Standards ensure that a cohesive design language is used throughout the public realm of the Diamond District; one that celebrates the District as its own unique destination. The development of the Design Standards was a collaborative effort between the City of Richmond's Project Management Team (PMT) made up of representatives from various City departments, design consultants, members of the Diamond District development team, and the public. The process of creating these Standards began with an extensive site walk with stakeholders to assess the existing site conditions and discuss a vision for the public realm. A review of the City of Richmond's existing streetscapes was undertaken to better understand those elements that make streets successful. Public engagement during the development of the Standards included in person and online public input sessions. The public was given an opportunity to provide feedback and vote on various public realm design options including options for landscape, hardscape, streetscapes, and site furnishings. Throughout the process, the PMT reviewed and provided input on drafts of the Standards as they were being developed.

The result of this collaborative effort is a set of standards that:

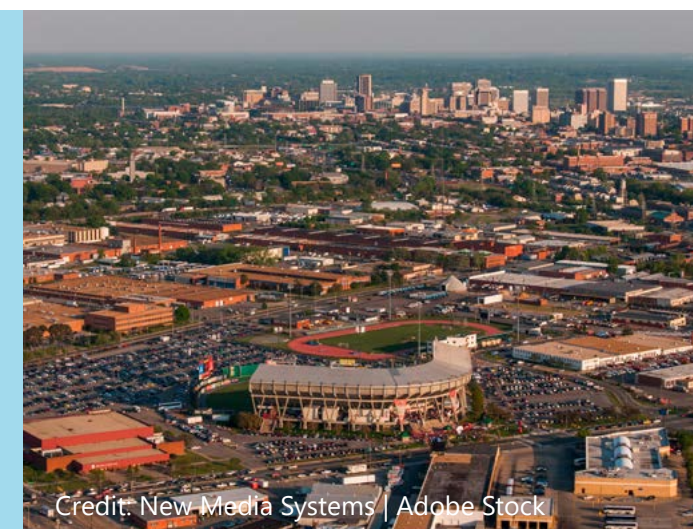
- Create a hierarchy of streets for the Diamond District
- Place a focus on providing safe multi-model circulation routes to and through the Diamond District
- Emphasize the incorporation of sustainable design practices
- Propose a design language that is both unique to the Diamond District yet rooted in the City of Richmond
- Promote a vibrant street life and encourages a healthy lifestyle

The chapters of the Diamond District Public Realm Design Standards address: the background of this unique development, the street network and streetscape including the proposed street hierarchy, public realm elements, paving and surface materials, landscape, and site furnishings. The appendix includes an overview of existing site conditions and case study findings.

The Diamond District Public Realm Design Standards apply to all spaces that fall within the public right-of-way of the Diamond District as well as publicly owned open space within the Diamond District. The boundaries of the Diamond District are show on the Diamond District Project Boundary Map on the following page. The Standards to not apply to development on private property, including the baseball stadium, although they should be used to inform design decisions on private property.



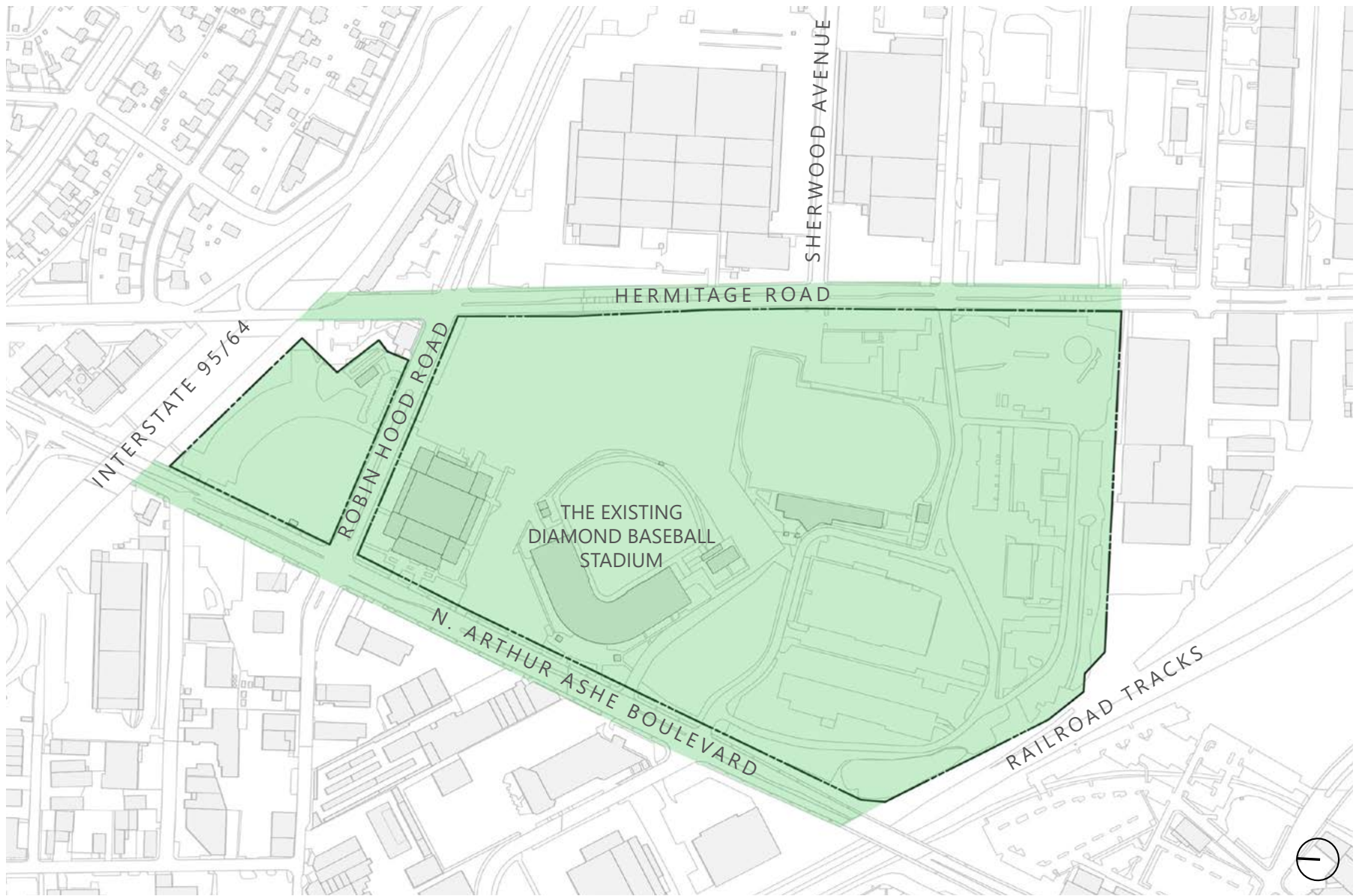
Credit: Crescent Heights



Credit: New Media Systems | Adobe Stock

Diamond District Project Boundary Map

The Diamond District Public Realm Design Standards shall apply to the public right-of-way and publicly owned open spaces that fall within the Diamond District Project Boundary



- DIAMOND DISTRICT PROJECT BOUNDARY
- DIAMOND DISTRICT EXISTING PARCELS BOUNDARY



Credit: San Francisco Municipal Transportation Agency



Credit: Richard Macdonald | Richmond Magazine



Credit: CallisonRTKL

2 | INTRODUCTION



2.1 BACKGROUND

The neighborhoods surrounding the Diamond District site have seen a substantial increase in investment over the past several years—with more investment in the works. In late 2021, the City of Richmond issued a Request for Information (RFI) to redevelop the Diamond District itself. This 66.7 acre underdeveloped, publicly owned property was to be redeveloped into a mixed-use, mixed income urban destination anchored by a regional baseball stadium. An emphasis was placed on creating a place where visitors could arrive safely on foot, bike, car, or transit. The new development was to include a signature linear park that would connect visitors and residents to a series of smaller parks and greenways nearby. Any proposal for this site was to be guided by the vision described in the Greater Scott's Addition Small Area Plan found in the City's comprehensive plan, Richmond 300: A Guide for Growth.

In September of 2022, the City of Richmond chose RVA Diamond Partners to be their development partner in redeveloping the Diamond District. The plan submitted by Diamond District Partners LLC is a mixed-income, mixed use community that features:

- A state-of-the-art baseball stadium that anchors the development
- A signature linear park meandering through the site with several distinct areas and programming planned for all Richmonders to enjoy throughout the year
- Trails, playgrounds, dog parks, and multiple outdoor gathering spaces
- Much needed pedestrian, bike, and vehicular connectivity throughout the site and into the surrounding neighborhoods
- A hotel
- Class A office space
- Retail
- Mixed income, multi-family residential housing

The City and RVA Development Partners desire to create vibrant, attractive, and safe streets and public spaces that will effectively connect people to the surrounding land uses and be frequented by residents and visitors to the Diamond District year round. It became apparent that it was necessary to develop these Diamond District Public Realm Design Standards to meet this goal and ensure that a cohesive design language is used throughout the Diamond District; one that identifies the Diamond District as its own unique destination while adding value to the surrounding neighborhoods and city.

2.2 SCOPE & APPLICATION OF DESIGN STANDARDS

The Diamond District Public Realm Design Standards are design requirements for spaces that fall within the public right-of-way and as well as publicly owned open spaces within the Diamond District. They should also be used to inform design decisions on private property. These standards shall be used by all stakeholders when deciding on elements when making design decisions for the public realm. The Design Standards present a vision for the public realm along with design options that allow for some flexibility and creativity. Designers should adhere as closely as possible to the recommendations made in the Design Standards. There may be instances when site constraints may require flexibility in applying the Design Standard. In those instances, every attempt should be made to meet the intent of those Design Standards.

2.3 OTHER APPLICABLE DOCUMENTS

The following City of Richmond documents provided guidance in developing the Public Realm Design Standards and should be used in conjunction with these standards:

- The City of Richmond Zoning Ordinance
- Richmond 300: A Guide for Growth
- Better Streets Manual
- Diamond District Transportation Study
- Richmond Sustainable Design Standards
- Urban Design Guidelines
- Sidewalk Design Guidelines
- City of Richmond Vision Zero
- Richmond Bicycle Master Plan
- Revealing Richmond: A Public Art Master Plan for the City of Richmond, Virginia
- Richmond Connects
- RVA Green 2050
- Fall Line Vision Plan

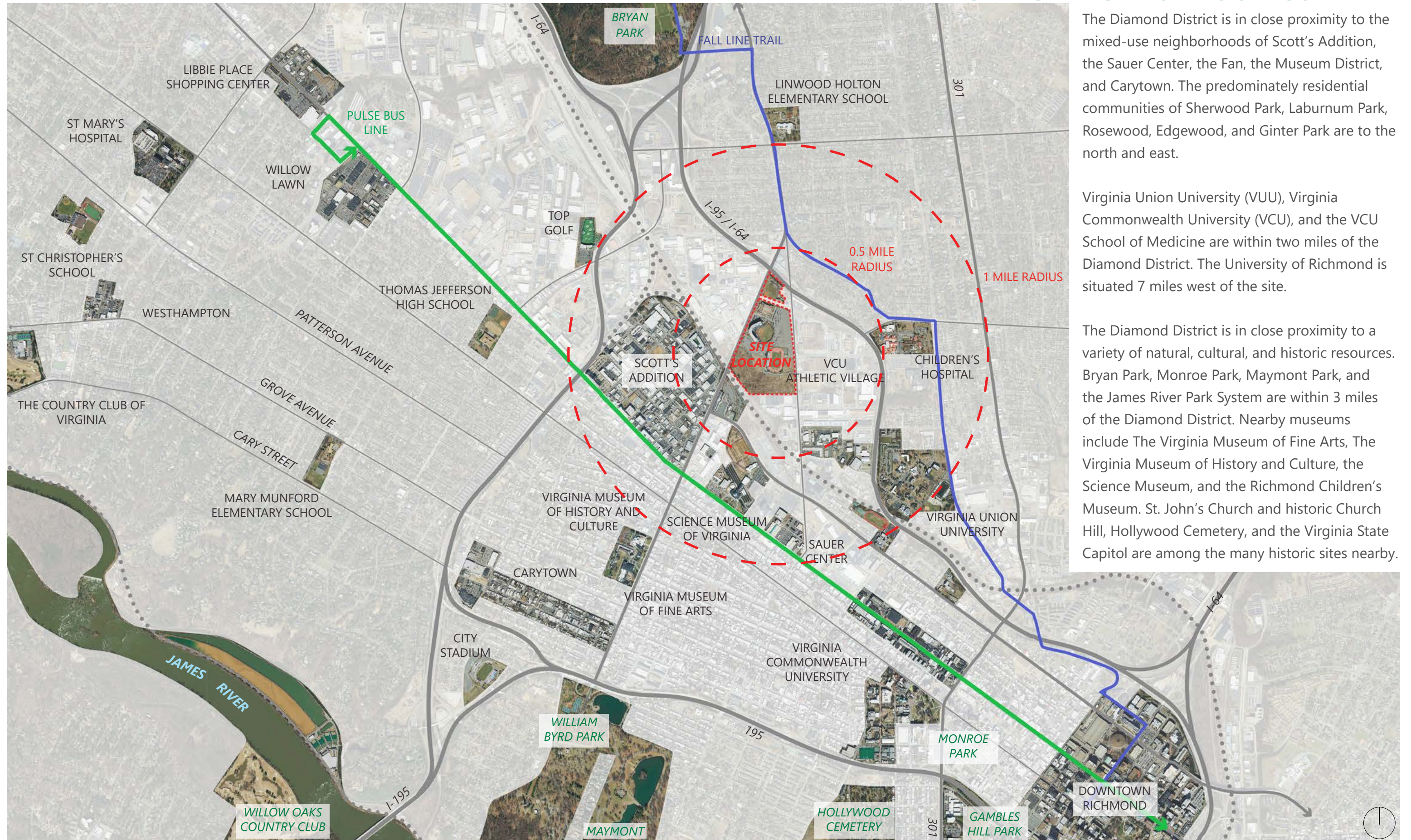
This is not an exhaustive list of all documents and regulations applicable to the Diamond District. The Diamond District Public Realm Design Standards are not a substitute for complying with codes and ordinances associated with the development review process.

2.3 SITE LOCATION



The Diamond District is a 66.7 acre site, centrally located within the City of Richmond and the greater Richmond Metropolitan Area making it an ideal location for a mixed-use, mixed-income urban live-work-play destination. Its superb central location offers unparalleled access to all that the City and region have to offer including employment opportunities, educational institutions, and destinations for recreation and leisure.

2.4 EXISTING NEIGHBORHOOD CONTEXT

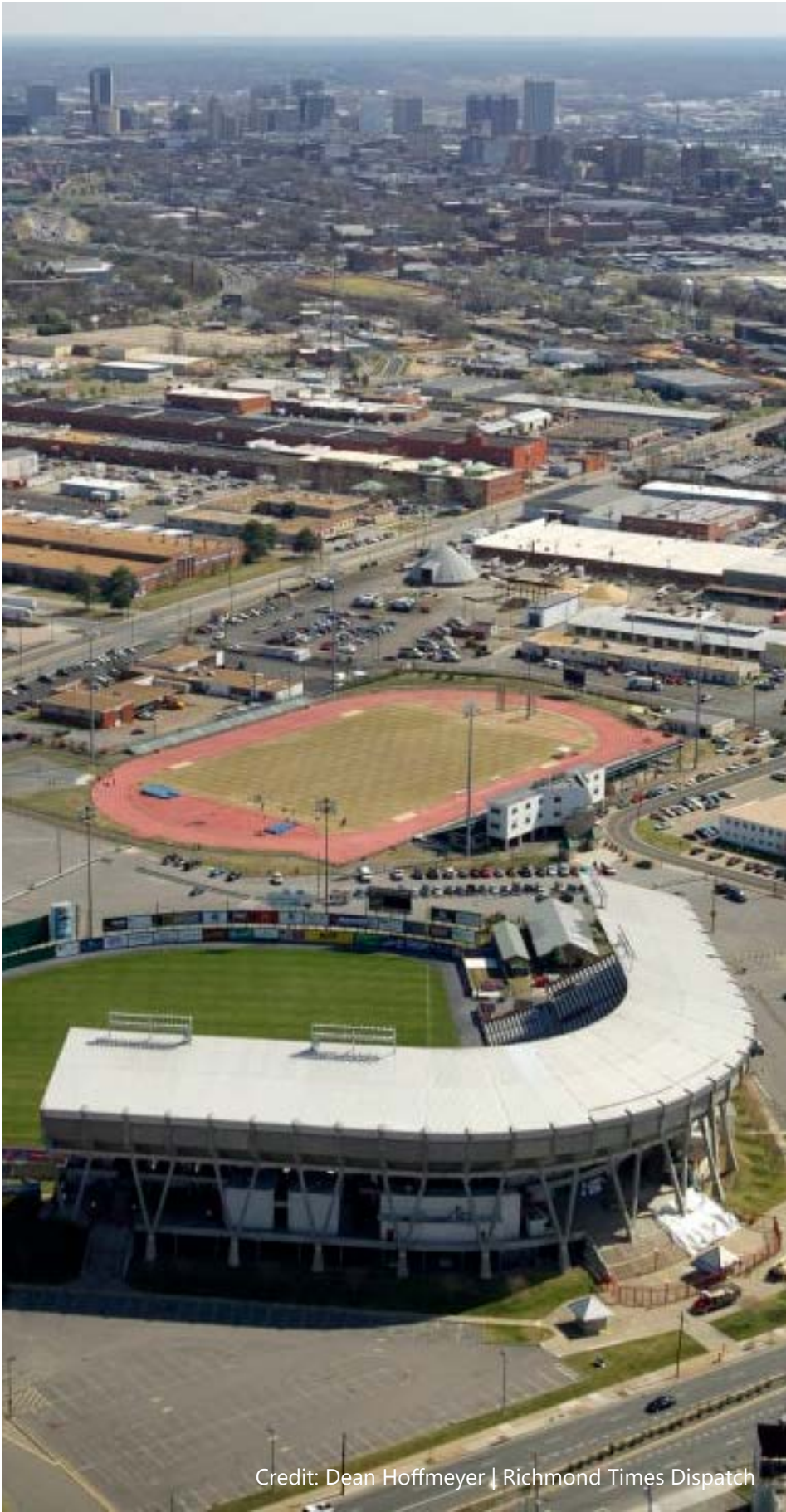


The Diamond District is in close proximity to the mixed-use neighborhoods of Scott's Addition, the Sauer Center, the Fan, the Museum District, and Carytown. The predominately residential communities of Sherwood Park, Laburnum Park, Rosewood, Edgewood, and Ginter Park are to the north and east.

Virginia Union University (VUU), Virginia Commonwealth University (VCU), and the VCU School of Medicine are within two miles of the Diamond District. The University of Richmond is situated 7 miles west of the site.

The Diamond District is in close proximity to a variety of natural, cultural, and historic resources. Bryan Park, Monroe Park, Maymont Park, and the James River Park System are within 3 miles of the Diamond District. Nearby museums include The Virginia Museum of Fine Arts, The Virginia Museum of History and Culture, the Science Museum, and the Richmond Children's Museum. St. John's Church and historic Church Hill, Hollywood Cemetery, and the Virginia State Capitol are among the many historic sites nearby.

2.5 EXISTING SITE CONDITIONS & SURROUNDINGS



Credit: Dean Hoffmeyer | Richmond Times Dispatch



Credit: Crixell Matthews | VPM News



Credit: Daniel Sangjib | Richmond Times Dispatch



The Diamond District site is comprised of 7 publicly owned parcels of land for a total of 66.7 acres as well as N. Arthur Ashe Boulevard, Hermitage Road, and Robin Hood Road where these streets are adjacent to the parcels. The underdeveloped site is currently home to a Diamond baseball stadium, the Arthur Ashe Jr. Athletic Center, Sports Backers Stadium, a recreational baseball field, and Parker Field Annex Recycling Center. A large amount of the site is either vacant or asphalt surface parking lots. The City of Richmond plans to relocate city functions, demolish the current baseball stadium and other buildings, and remediate the Diamond District site to prepare for the proposed development.



DIAMOND DISTRICT PARCELS

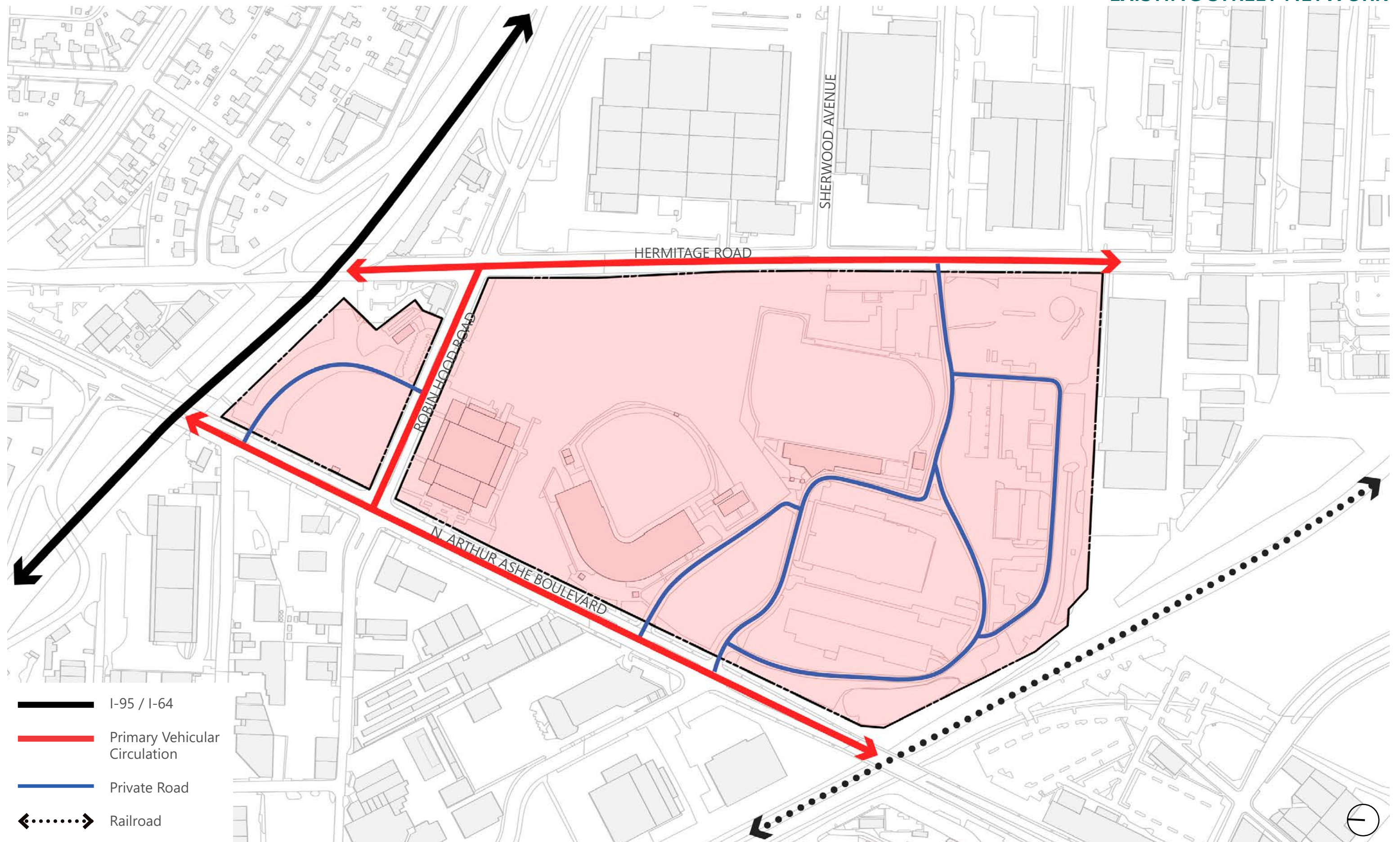
RECENT & FUTURE DEVELOPMENT

- 1 Scott's Walk; 16,000 SF of retail and restaurant
- 2 1801 Ellen Road; 2 story emergency center, 15,000 SF
- 3 2902 N Arthur Ashe Boulevard; NOVEL Scott's Addition. 272 apartments
- 4 1117-1209 N Arthur Ashe Boulevard; The Ace 295 apartment building with 13,000 SF of commercial space
- 5 2700 W Leigh Street; 375 Unit, 5 story apartment building
- 6 2300 Hermitage Road - Duplex Station on Hermitage. 3 story, 142 apartment building with 24,000 SF of office
- 7 1613 Ownby Lane; The Porter 5 story, 314 unit apartment building
- 8 The Cooperage Apartments; 150+ apartment units
- 9 VCU Athletic Village
- 10 The Park RVA
- 11 1415 - 1601 Rhoadmiller Street 4 story, 180 unit condo building
- 12 Stylecraft Homes; The Outpost at Brewers Row
- 13 Tommy's Car Wash
- 14 375 Unit Multi-Family Development







DIAMOND DISTRICT PARCELS



DIAMOND DISTRICT PARCELS



EXISTING SITE UTILITIES AND
BUS ROUTES

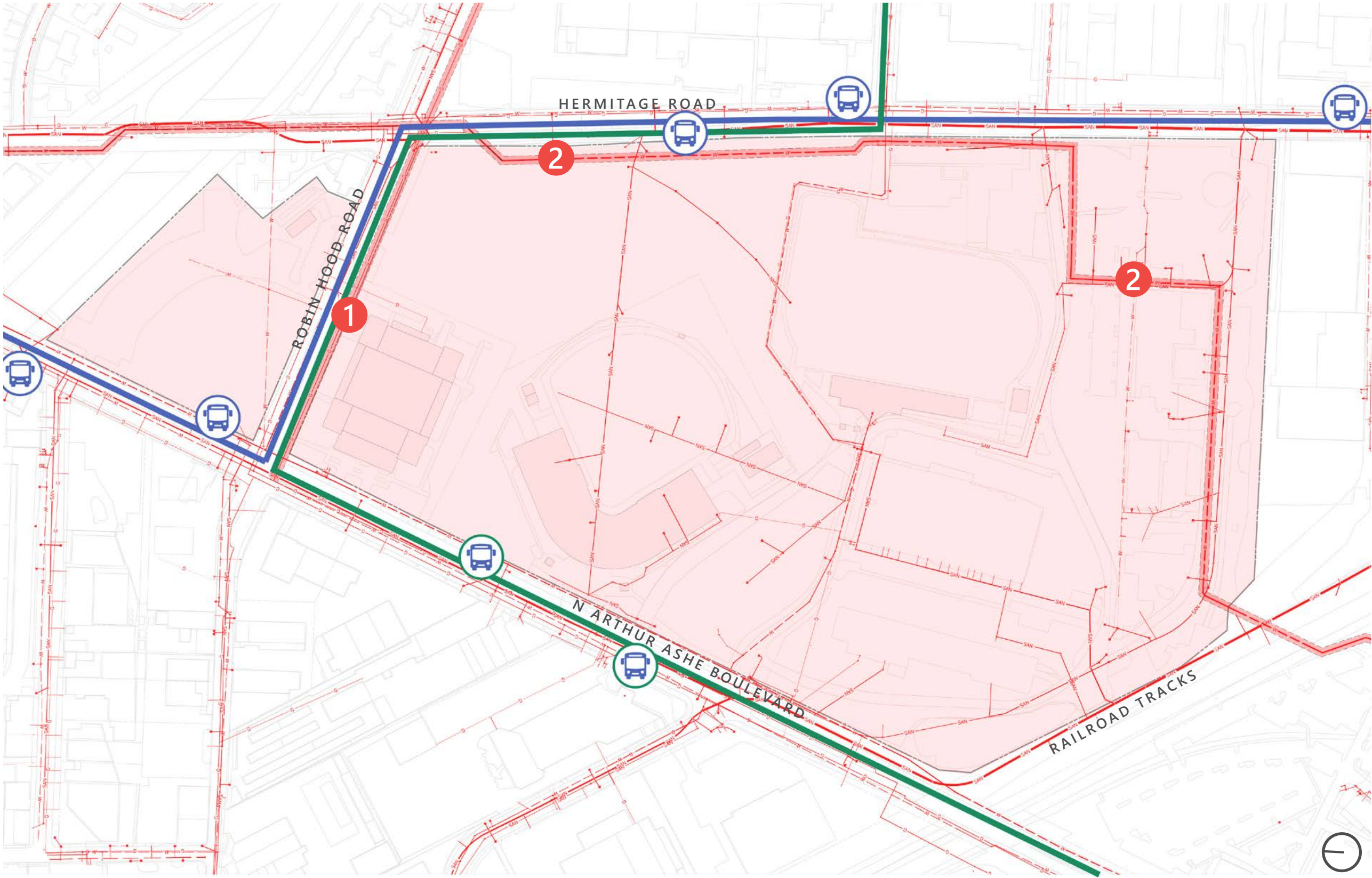
-  BUS STOPS ROUTE 14
-  BUS ROUTE 14
-  BUS STOP ROUTE 20
-  BUS ROUTE 20
-  UTILITY
-  UTILITY EASEMENT
- 1

20' HIGH PRESSURE GAS PIPELINE EASEMENT
- 2

20' WATERLINE EASEMENT

Bus Routes:
The two existing bus routes adjacent to the Diamond District Site, Routes 14 and 20, will likely experience an increase in patrons from the development of the Diamond District.

Utilities:
Multiple utilities are located within and adjacent to the site. Of note, an active high pressure gas pipeline within a 20' easement runs along the southside of Robin Hood road and cannot be encroached upon. A 36" water line within a 20' easement runs along Hermitage Road and through the site. This easement will be relocated off site.



2.6 VISION FOR THE DIAMOND DISTRICT PUBLIC REALM

DIAMOND DISTRICT CONCEPTUAL SITE PLAN

SITE PLAN IS CONCEPTUAL IN NATURE AND SUBJECT TO CHANGE.



The Diamond District will be a vibrant, authentic Richmond neighborhood and one-of-a-kind ballpark experience that celebrates creative placemaking, sport, art, culture, and culinary excellence. The District will have a strong sense of place with a balance of well designed buildings interspersed with open space, walkable tree lined blocks, and sustainable features. Having a well designed public realm, including a thoughtfully planned system of streets and public spaces and a cohesive design language, is vital to creating a vibrant, thriving neighborhood imbued with a sense of place. The public realm should not only be safe and functional, but also a beautiful place to be in and move through; a place that attracts businesses, residents, and visitors to the Diamond District. It is envisioned that the Diamond District public realm will:

- Have an active street life with spaces, amenities, and land uses that encourage a variety of active and passive activities along the street year round including dining, shopping, strolling, exercising, and socializing.
- Consist of a well designed multi-model circulation network that safely and efficiently moves pedestrians, bicyclists, and vehicular traffic. This system should effectively connect users to the baseball stadium, businesses, residential buildings, and open spaces within the Diamond District and to the surrounding neighborhoods beyond.
- Be accessible, safe, and comfortable for a wide variety of age groups, abilities, and users.
- Meet the various and differing needs of full time residents, business owners, employees, and visitors.
- Include landscaping, hardscape, and site furnishings that define and beautify the streets and gathering spaces.
- Advance the City of Richmond's stated goal to be the greenest city on the East Coast by incorporating sustainable design practices that improve the environment and health of the community.
- Consist of a design language that is both unique to the Diamond District yet rooted in the City of Richmond.
- Enhance the surrounding architecture and thoughtfully respond to the scale, character and nature of surrounding land uses.
- Encourage a healthy lifestyle.

3 | STREET NETWORK & STREETScape STANDARDS

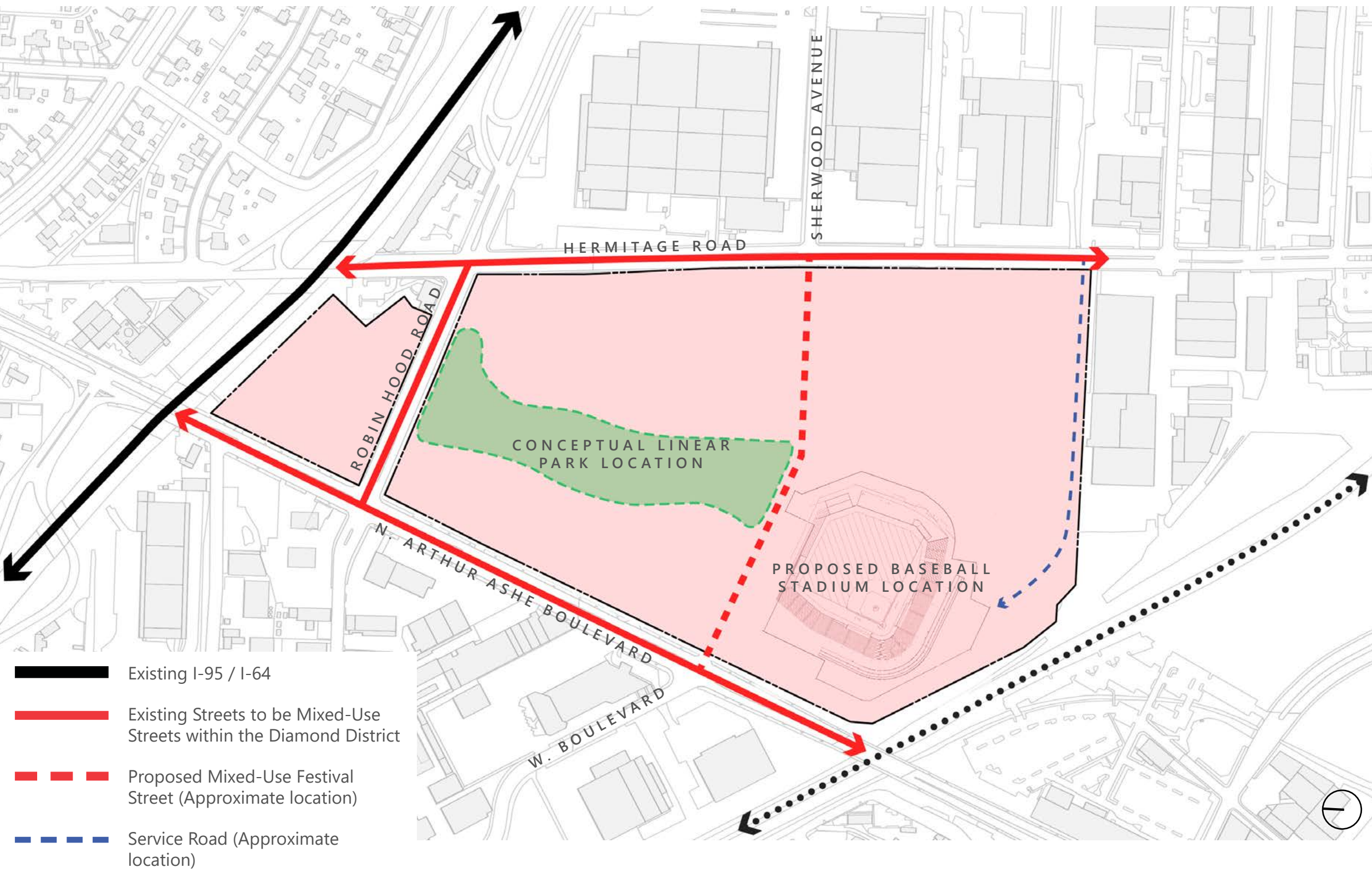
3.1 CHAPTER INTRODUCTION

The City of Richmond’s Complete Streets Initiative as expressed in the City’s Better Streets Manual strives to ensure the creation of a comprehensive, integrated, connected, multimodal transportation network that balances the accessibility, mobility, health, and safety needs of all users, including persons with disabilities, seniors, children, and families. Streets are an essential component of livable, attractive neighborhoods. A well planned network of streets within the Diamond District will safely accommodate the varying needs of pedestrians, bicyclists, drivers, and transit users, while providing connectivity and access to adjacent land uses, open spaces, and amenities. Special attention should be given to designing streets that are not only used for travel, but streets which are beautiful spaces that encourage a variety of activity on the street. Thoughtfully designed streetscapes can help define the character of this place, add economic value, and enhance the quality of life and health for residents, businesses, and visitors.



CONCEPTUAL STREET NETWORK

Site plan is conceptual in nature and subject to change. Future roads internal to the Diamond District are not shown.



The street network for the Diamond District shall establish a hierarchy of streets. N. Arthur Ashe Boulevard, Hermitage Road, Robin Hood Road shall function as Mixed-Use Streets* as described in the Better Streets Manual. A proposed Festival Street will extend through the site and provide access to the ballpark. The Festival Street will also be a Mixed-Use Street typology. A network of internal Neighborhood Residential Streets* and Linear Park Streets* will create walkable neighborhood blocks that support local pedestrian, bicycle, and vehicular circulation. A service road along the south of the site will provide access to the baseball stadium for deliveries. A public pedestrian mews that weaves between multi-family residential buildings on the east side of the site shall provide a secondary north south pedestrian and bicycle route through the Diamond District in addition to the linear park.

*For further information on street typologies referenced in these Design Standards, see the Better Streets Manual.

3.3 N. ARTHUR ASHE BOULEVARD

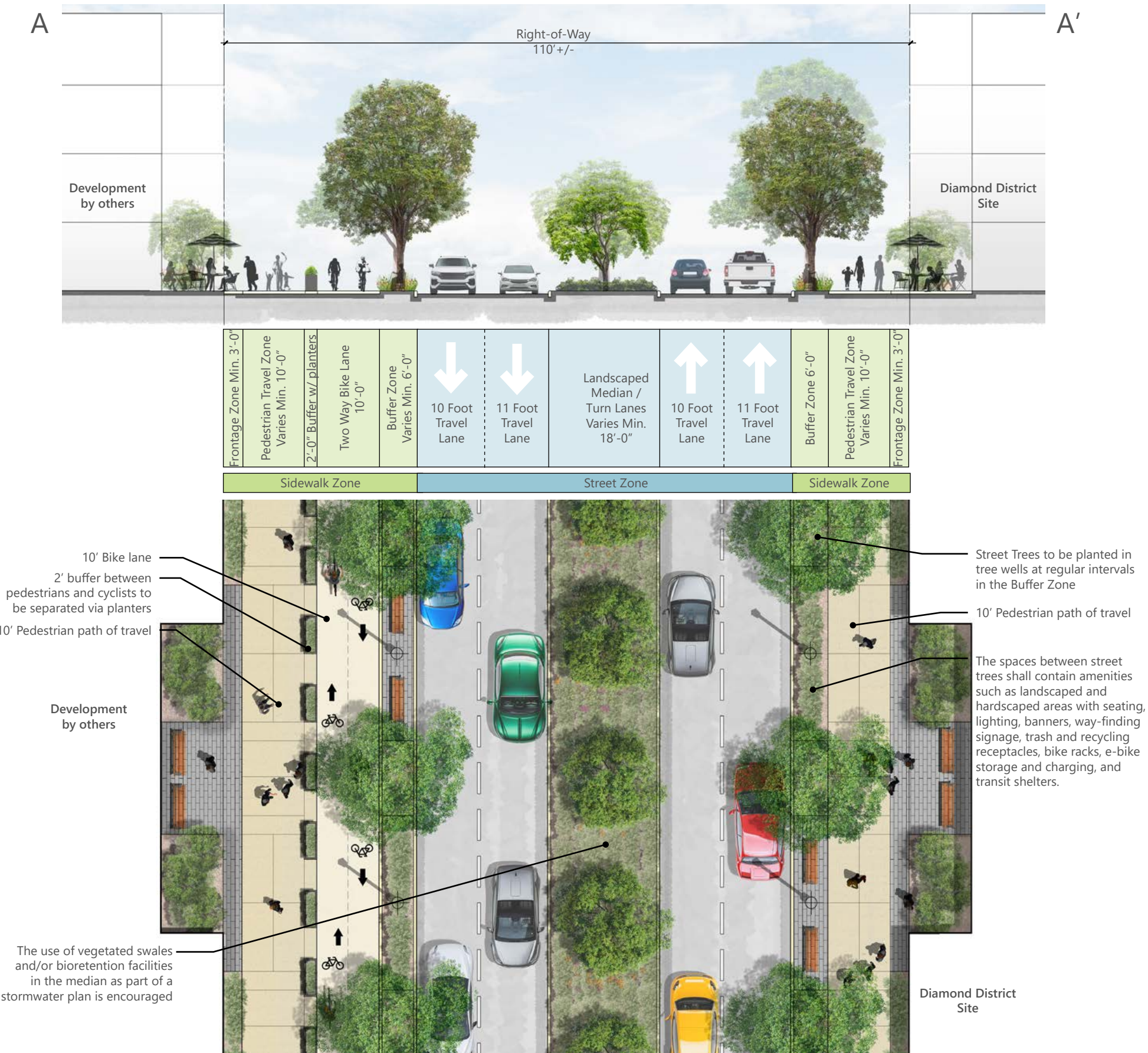
Over the last decade, redevelopment and new development close to the street has changed the character of N. Arthur Ashe Boulevard adjacent to the Diamond District, slowly transforming it into a true mixed use boulevard. Within the Diamond District, the new baseball stadium, as well as multi-family residential, a hotel, retail, and office land uses will be situated along this street. N. Arthur Ashe Boulevard, along with Hermitage Road, is one of the primary vehicular circulation routes to and around the Diamond District, connecting the District to other city neighborhoods. N. Arthur Ashe Boulevard provides access to and from Interstate 95 North / 64 West just north of the Diamond District. The Richmond 300 Plan calls for N. Arthur Ashe Boulevard to become a “Great Street” featuring buildings that address the street, underground utilities, street trees, lighting, enhanced transit, and other amenities. The Richmond 300 Plan calls for it to be a high frequency transit route with multiple stops. Additionally, plans are underway to replace the bridge over the railroad tracks just south of the Diamond District. A shared use path is proposed for west side of the new bridge. The pedestrian travel zone and bike lanes in Conceptual Section & Plan A shall tie into the shared use path.

- Better Streets Manual Street Typology: Mixed-Use Street
- Existing Right-of-Way Width: Varies, but is predominately 110'-0"
- Proposed Right-of-Way Width: Existing to remain

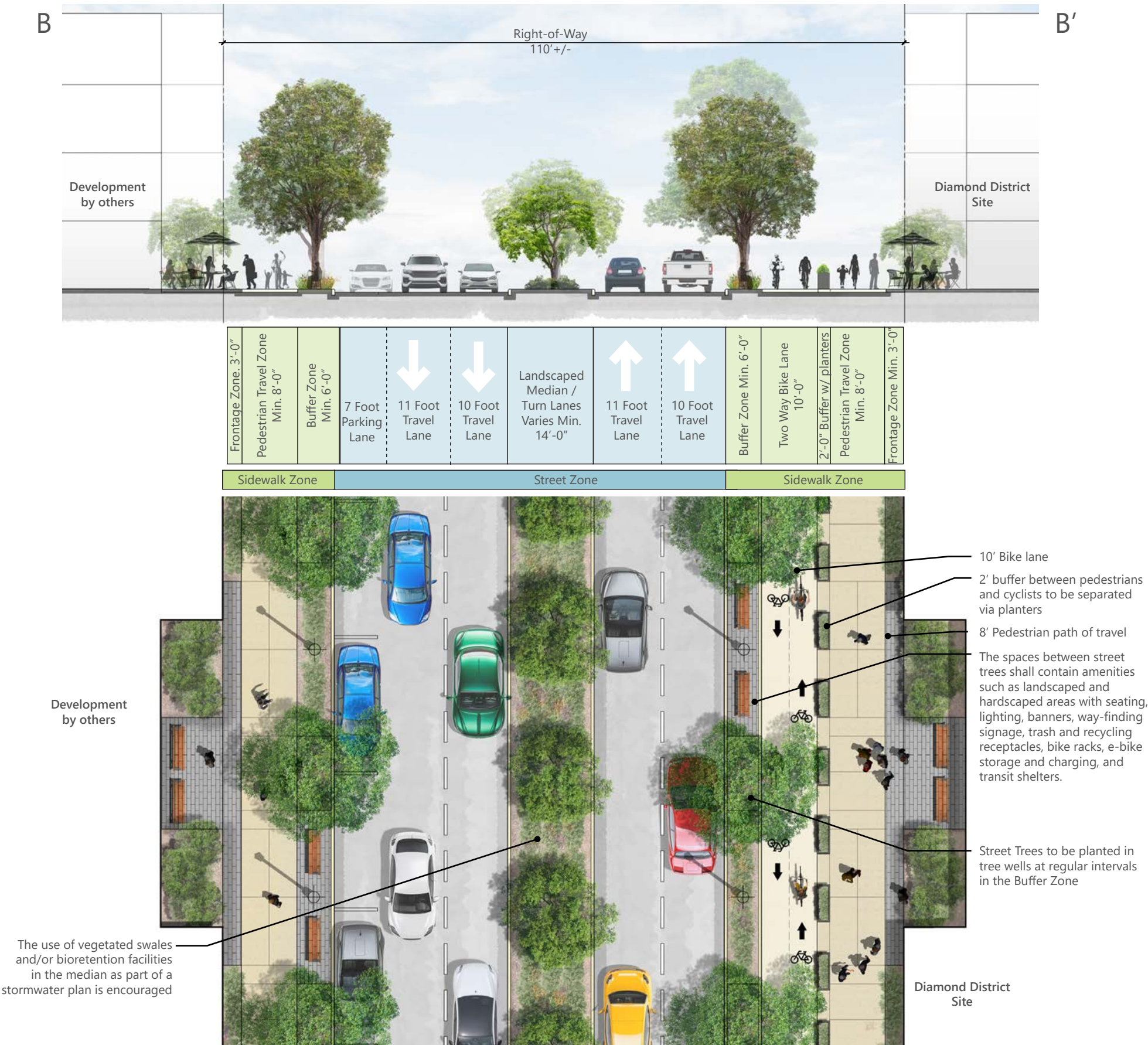
Street Location Map



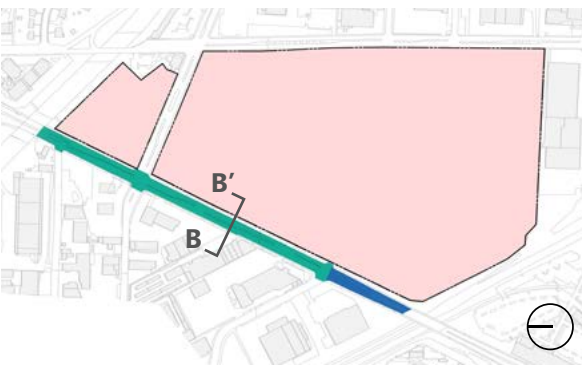
Conceptual Section & Plan A - N. Arthur Ashe Boulevard between the N. Arthur Ashe bridge and the Festival Street



Conceptual Section & Plan B - N. Arthur Ashe Boulevard between the Festival Street & Interstate 95



Street Location Map

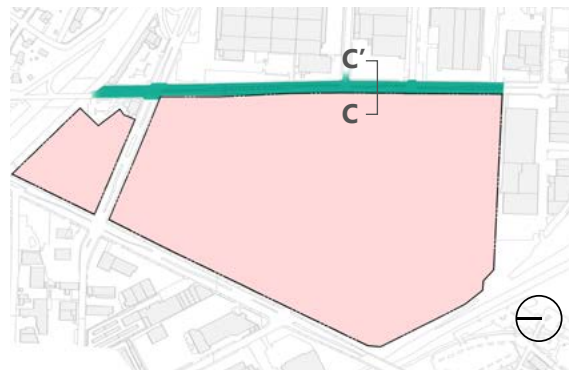


3.4 HERMITAGE ROAD

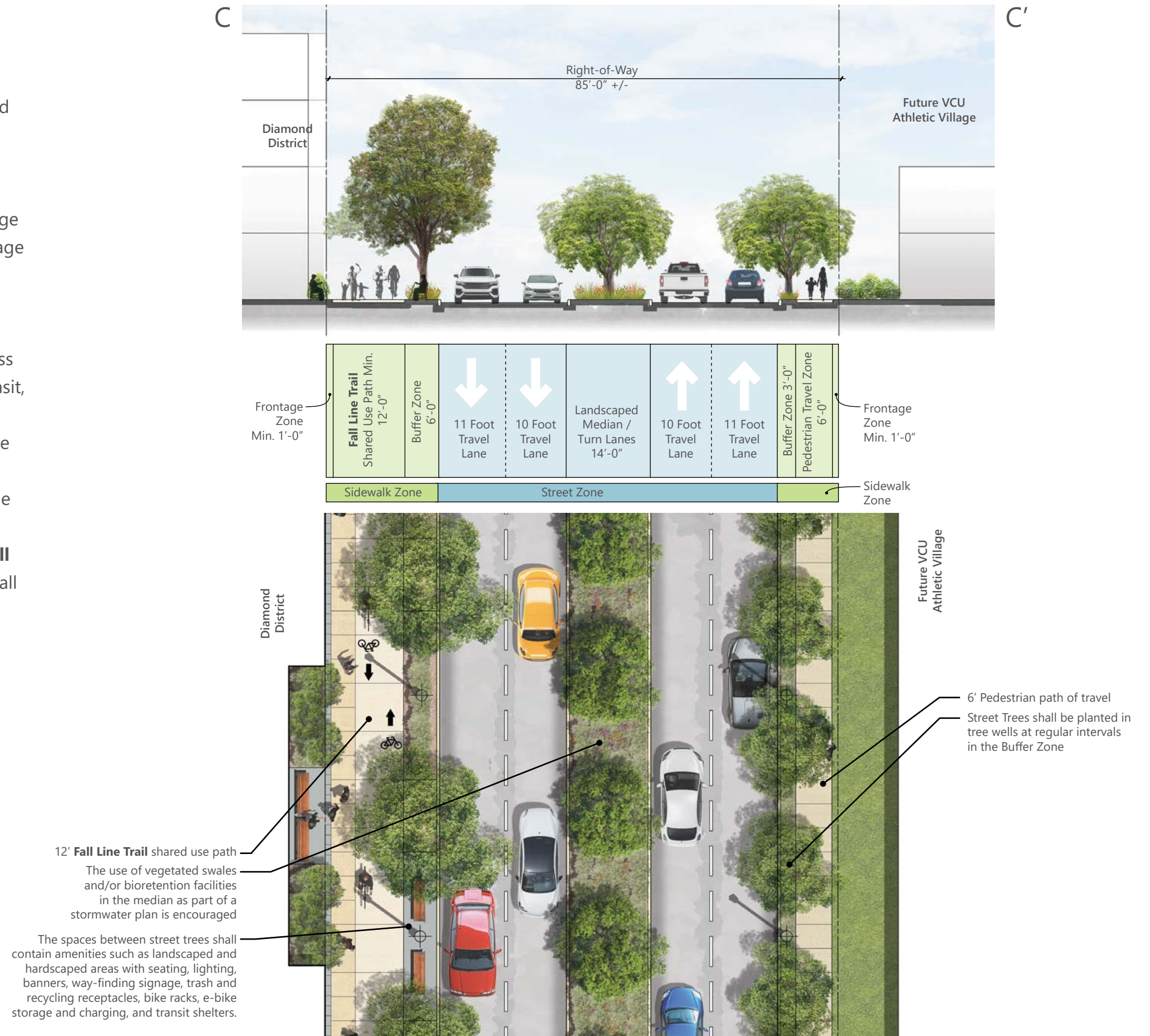
Hermitage Road, along with N. Arthur Ashe Boulevard, is one of the primary transportation circulation routes to and around the Diamond District. Hermitage Road near the Diamond District can be accessed from Interstate 95 South / 64 East. The Sauer Center commercial redevelopment at the intersection of Hermitage Road and W. Broad Street has started to transform and enliven the character of Hermitage Road south of the Diamond District. The proposed VCU Athletic Village as well as multi-family residential development within the Diamond District along Hermitage Road promises more transformation. The Richmond 300 Plan calls for Hermitage Road to be both a major mixed-use street and a "Great Street" featuring buildings that address the street, underground utilities, street trees, lighting, enhanced transit, and other amenities. The Richmond 300 also calls for it to be a high frequency transit route with multiple stops. Hermitage Road from the city line south to W. Broad Street, which encompasses the stretch of road through the Diamond District, was identified by the City and the Richmond Bicycle Master Plan project team as a key connection in need of bicycle infrastructure improvements. **The Fall Line Trail shall be located along the west side of Hermitage Road.** Refer to the Fall Line Vision Plan for additional design standards and guidance.

- Better Streets Manual Street Typology: Mixed-Use Street
- Existing Right-of-Way Width: Varies, 85'-0" +/-
- Proposed Right-of-Way Width: Min. 85'-0"

Street Location Map

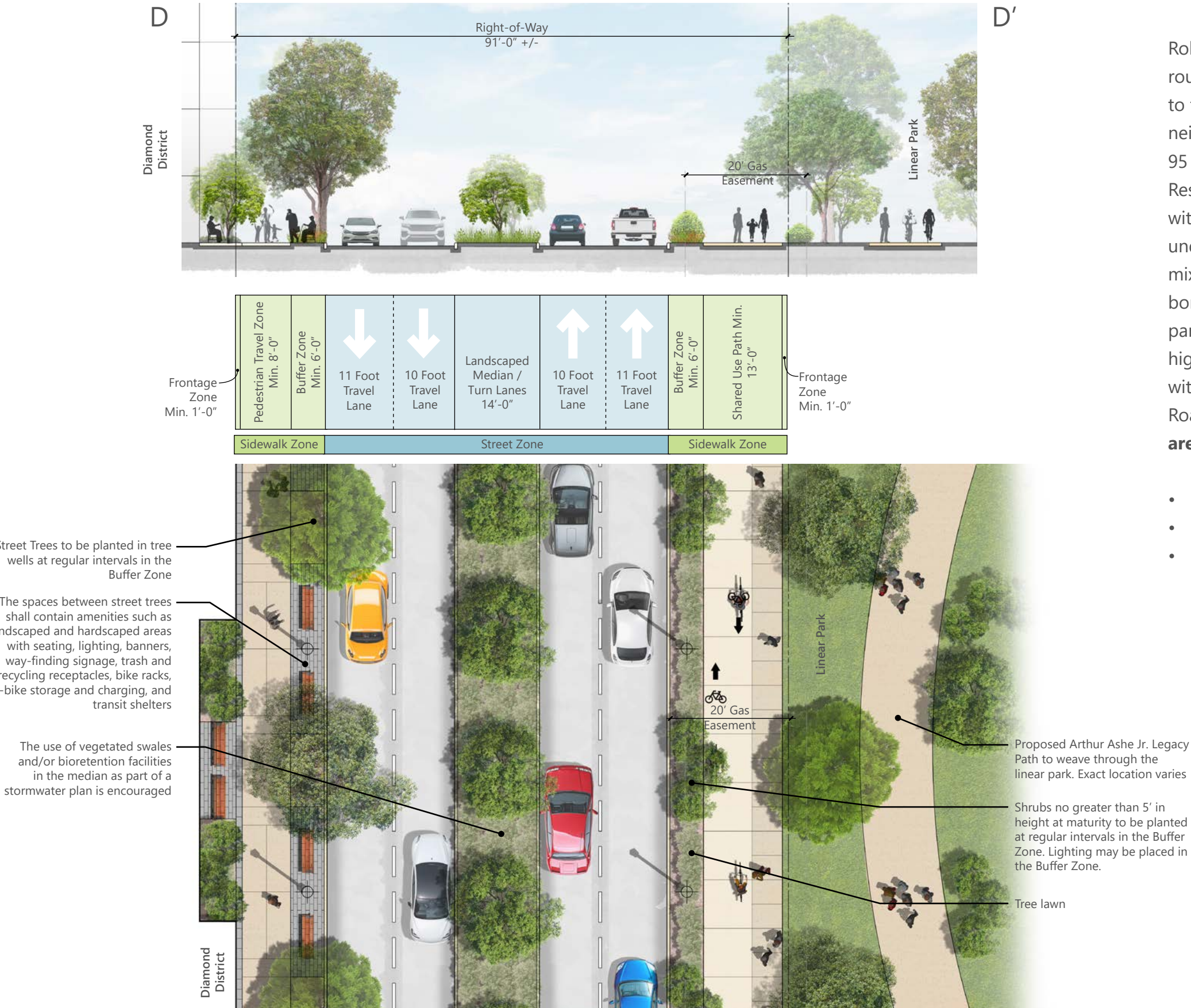


Hermitage Road Conceptual Section & Plan



3.5 ROBIN HOOD ROAD

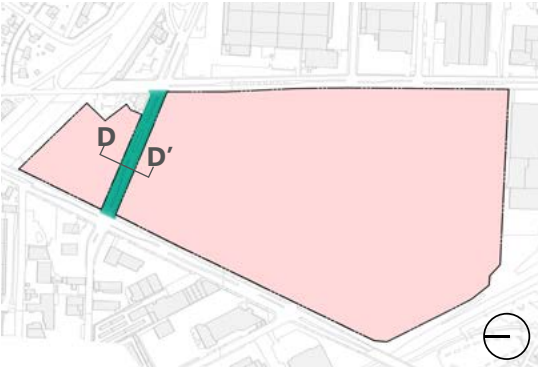
Robin Hood Road Conceptual Section & Plan



Robin Hood Road is one of the primary transportation circulation routes to and through the Diamond District, connecting the District to the rest of Greater Scott’s Addition and various Northside neighborhoods. Robin Hood Road provides direct access to Interstate 95 South / 64 East as well as the proposed VCU Athletics Village. Residential and commercial development along Robin Hood Road within the Diamond District will begin to transform this route from an underdeveloped street marked with vast asphalt parking lots into a mixed use corridor. The northern edge of the proposed linear park will border Robin Hood Road, making Robin Hood Road a gateway into the park. The Richmond 300 Plan designates Robin Hood Road as a future high frequency transit route. An active high pressure gas pipeline within a 20’ easement runs parallel to the south side of Robin Hood Road. **No plantings or shrubs greater than 5’ in height at maturity are permitted within the easement.**

- Better Streets Manual Street Typology: Mixed-Use Street
- Existing Right-of-Way Width: Varies, 91'-0" +/-
- Proposed Right-of-Way Width: Existing to remain

Street Location Map



3.6 FESTIVAL STREET

The proposed Festival Street is expected to be a hub of activity in the Diamond District, with an increase in activity occurring on game days. The Festival street may be closed from time to time for special events, especially when there is a game at the ballpark. Pedestrian access to the ballpark will be from the Festival Street. Additionally, commercial development, including retail shops and a hotel, multi-family residential uses, and the linear park will front this street.

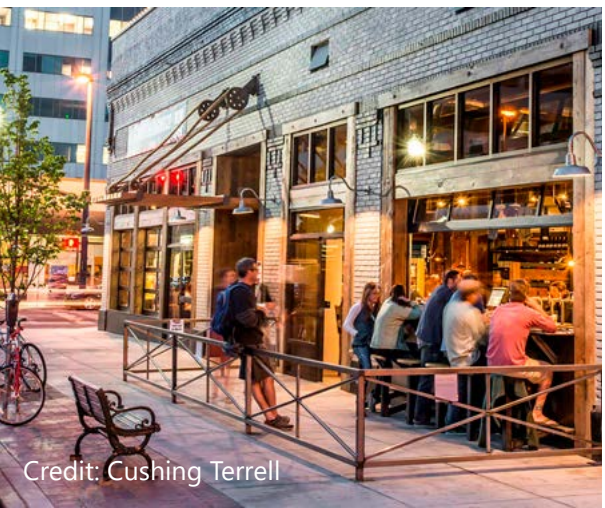
Consideration must be given to using design elements that designate the Festival Street as a special space within the Diamond District. String lights that transverse the street shall be included in the design. A street mural or stamped asphalt in the Street Zone shall be considered at key locations, particularly where the Linear Park borders the Festival Street. The Festival Street design shall include accommodations for food trucks and entertainment during special events, including space for food truck parking and electrical hook-ups. A plan shall be developed to safely and efficiently manage vehicular, pedestrian, and bicycle circulation when the Festival Street is closed for events.

A high visibility block-style crosswalk shall be installed all intersections. Stamped asphalt may be used for crosswalks at unsignalized intersections, especially where Linear Park Perimeter Streets intersect the Festival Street. Raised crosswalks shall be used where the public Mews intersects the Festival Street and where the Linear Park Perimeter Streets intersect the Festival Street.

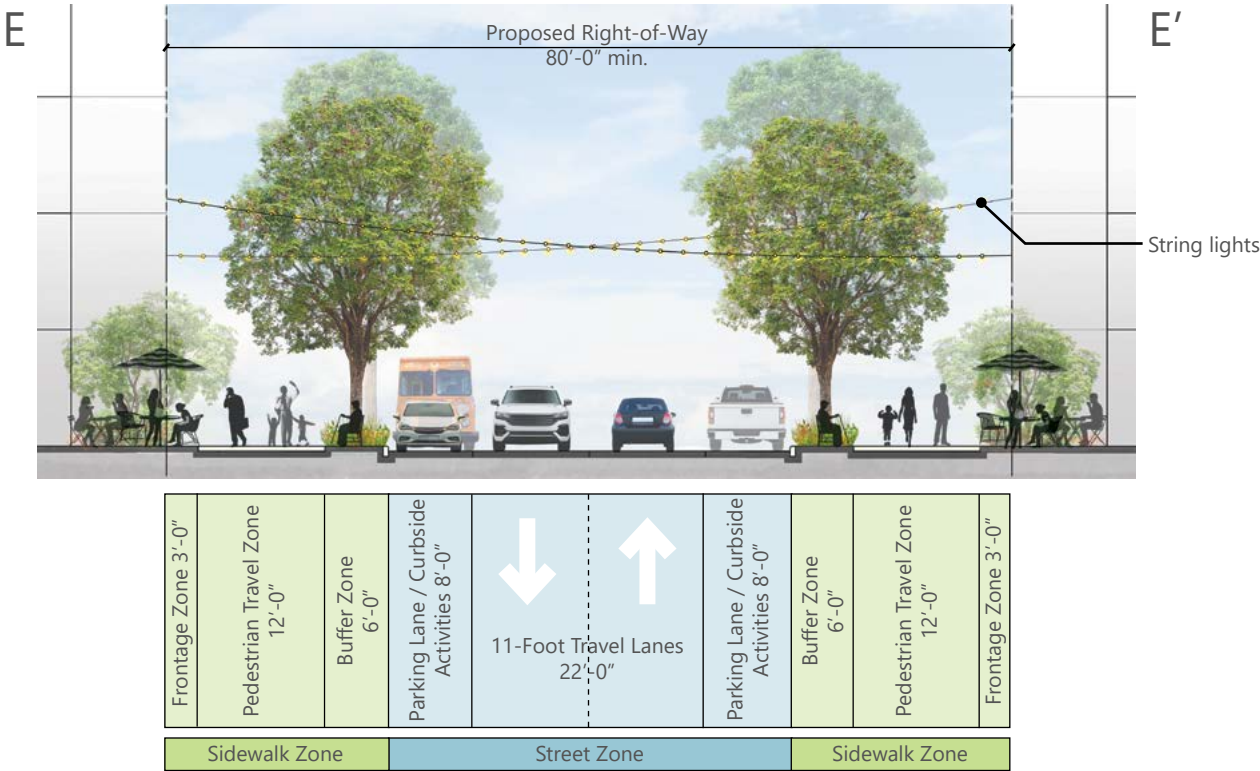
The design of the Festival Street must address how valet drop off can safety and efficiently occur at the hotel proposed for the intersection of the Festival Street and N. Arthur Ashe Boulevard. Additionally, a school bus drop off area shall be located on the Festival Street or adjacent to the Stadium Plaza. See 4.7 Public Gathering Space Considerations.

- Better Streets Manual Street Typology: Mixed-Use Street
- Proposed Right-of-Way Width: minimum 80’-0”

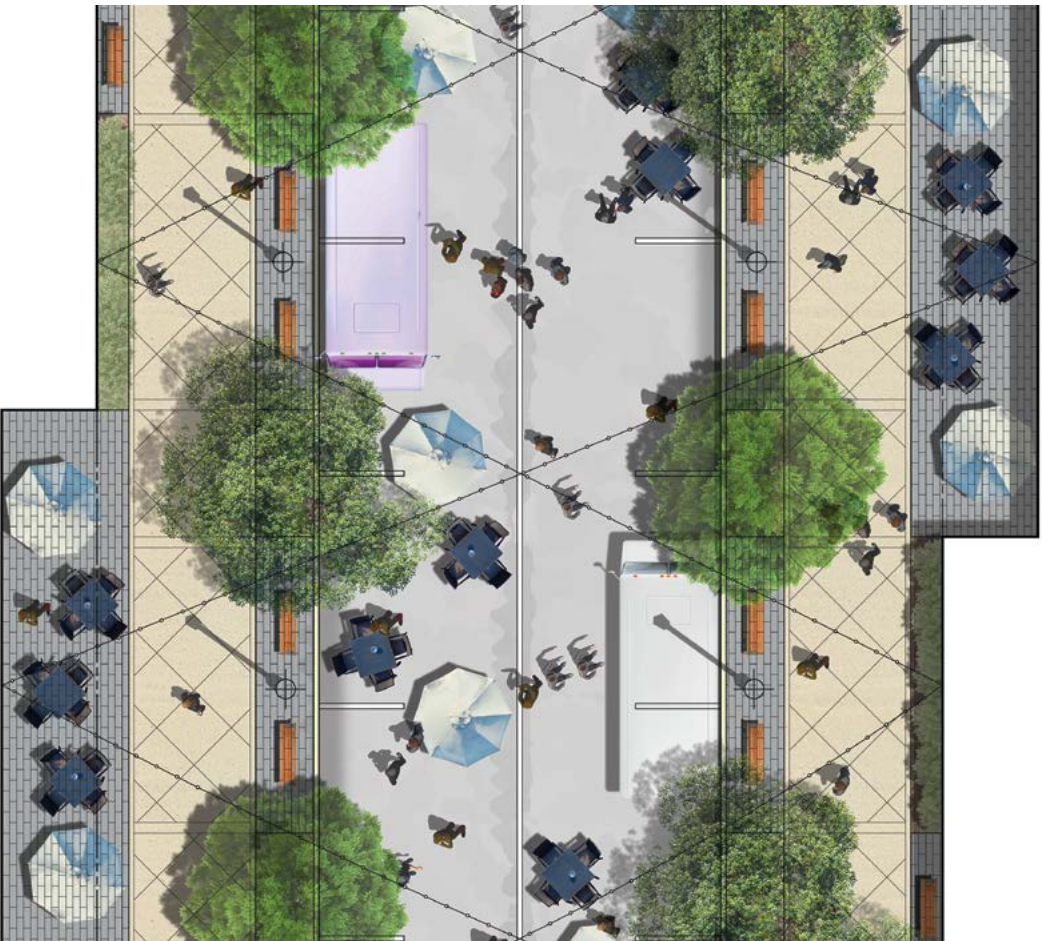
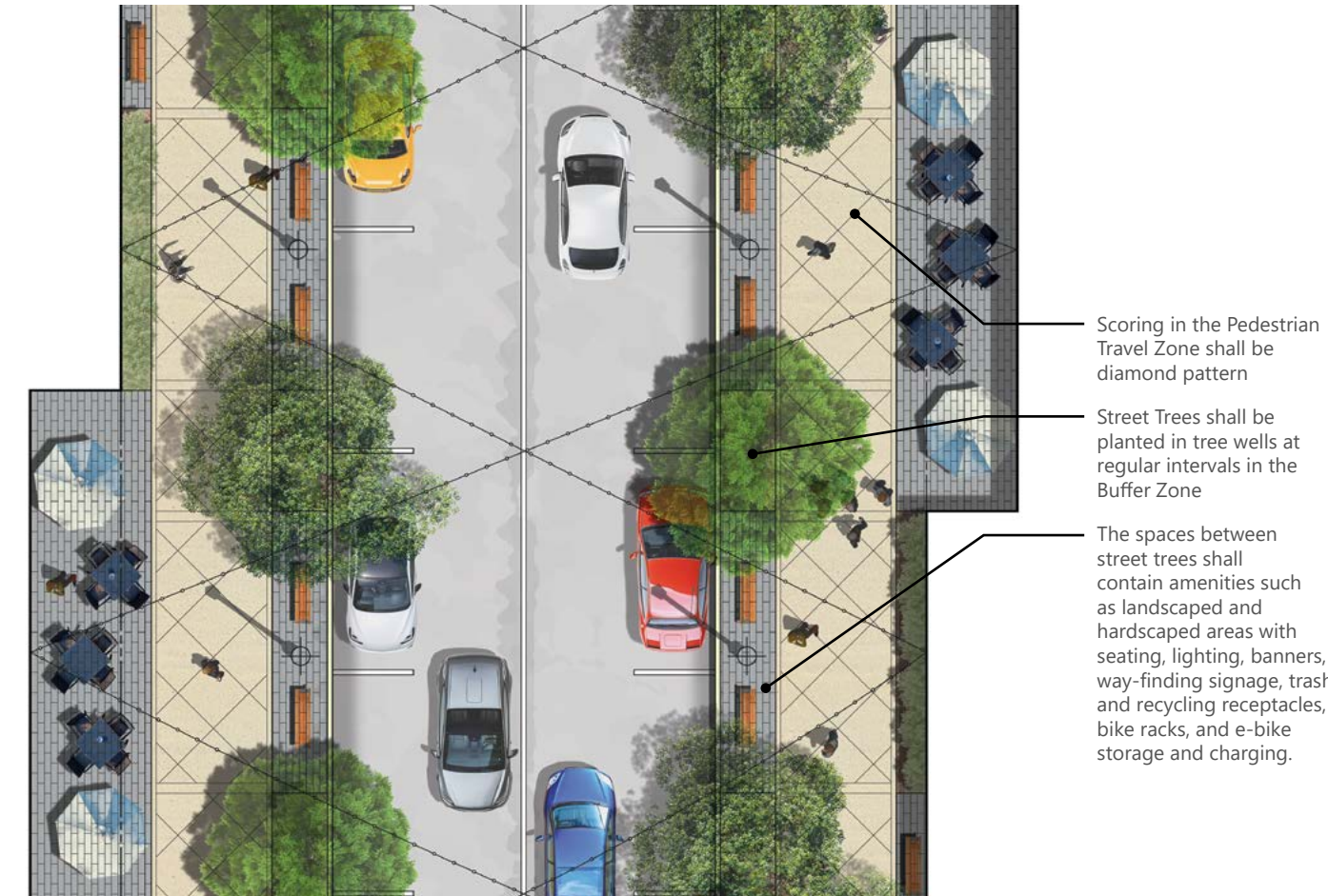
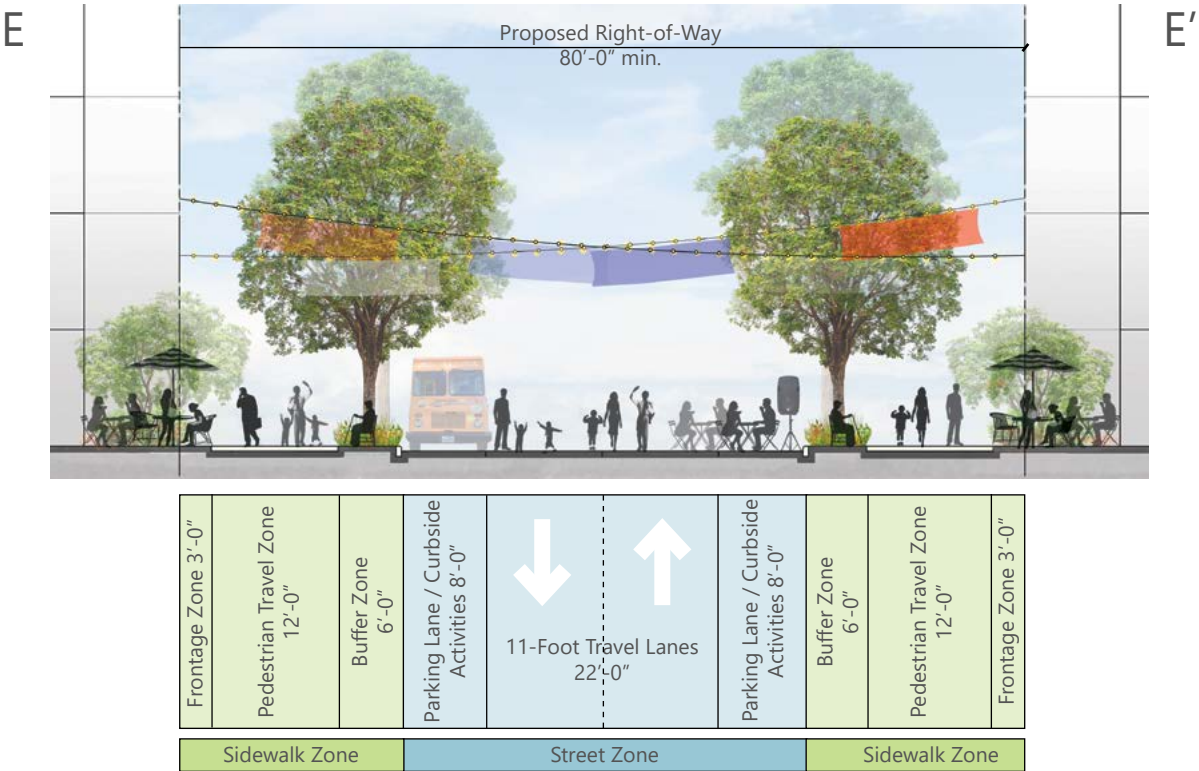
Festival Street Precedent Images



Festival Street Conceptual Section & Plan



Festival Street Conceptual Section & Plan when Closed for Events



Festival Street Approximate Location

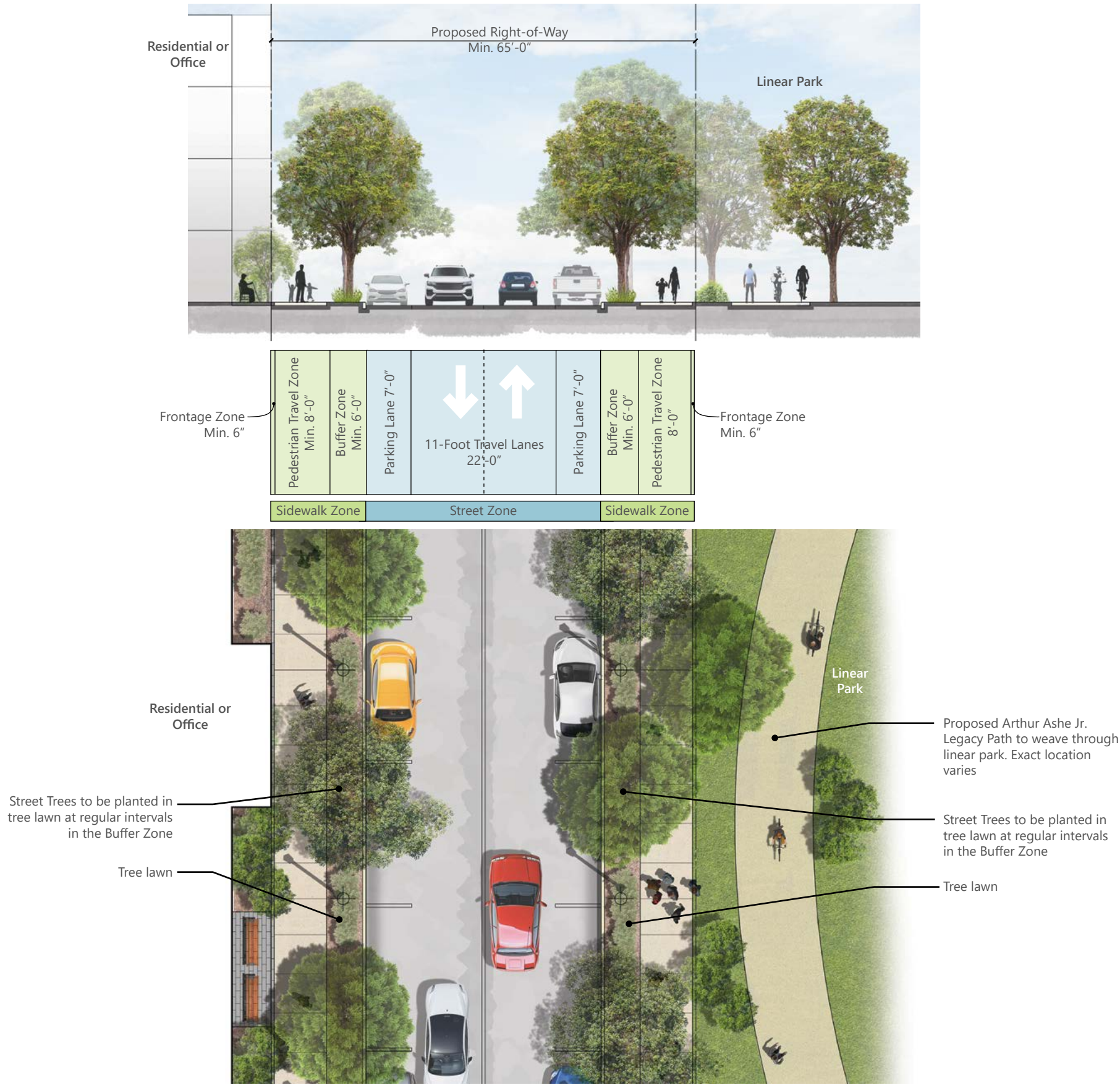


3.7 LINEAR PARK PERIMETER STREETS

The Linear Park Perimeter Streets will be internal to the Diamond District. They are unique in that on one side they will be bordered by the proposed linear park and on the opposite side, they will be bordered by multi-family residential or office land uses. They must accommodate the needs of residents as well as provide safe pedestrian access to this signature park. Tree lawns shall be used in the Buffer Zone rather than tree wells.

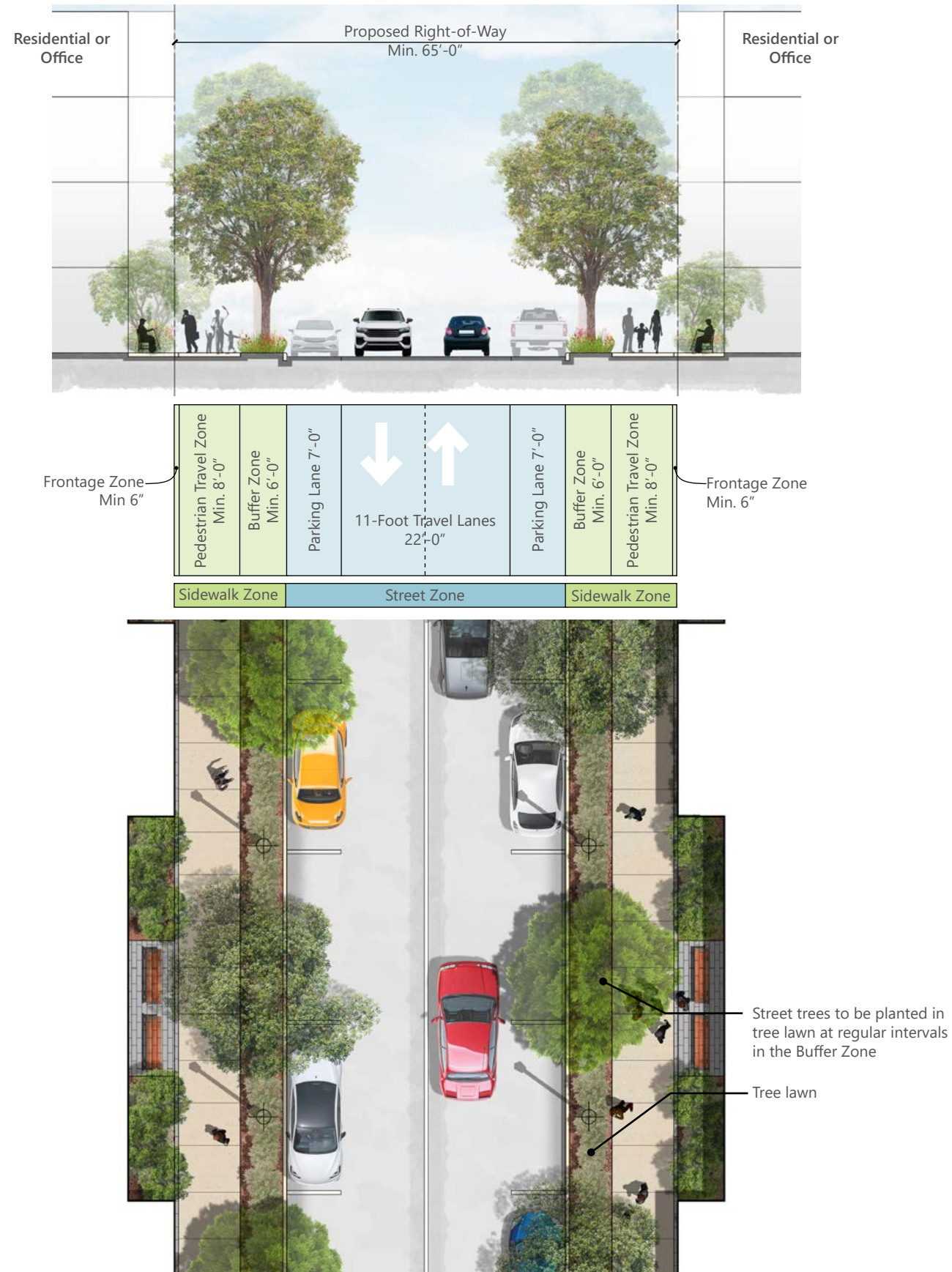
- Better Streets Manual Street Typology: These streets contain elements of Neighborhood Residential Streets and Parkways
- Proposed Right-of-Way Width: minimum 65'-0"

Linear Park Perimeter Streets Conceptual Section & Plan



3.8 NEIGHBORHOOD RESIDENTIAL STREETS

Neighborhood Residential Street Conceptual Section & Plan



Neighborhood Residential Streets will be internal to the Diamond District. They will be bordered on both sides by multi-family residential or office land uses. These streets will typically have lower levels of vehicular traffic and are not intended for cut through traffic. They will most often be frequented by people that reside in the neighborhood. Tree lawns shall be used in the Buffer Zone rather than tree wells.

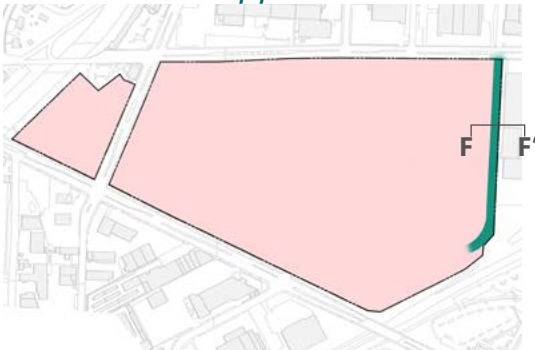
- Better Streets Manual Street Typology: Neighborhood Residential Street
- Proposed Right-of-Way Width: 65'-0"

3.9 SERVICE ROAD

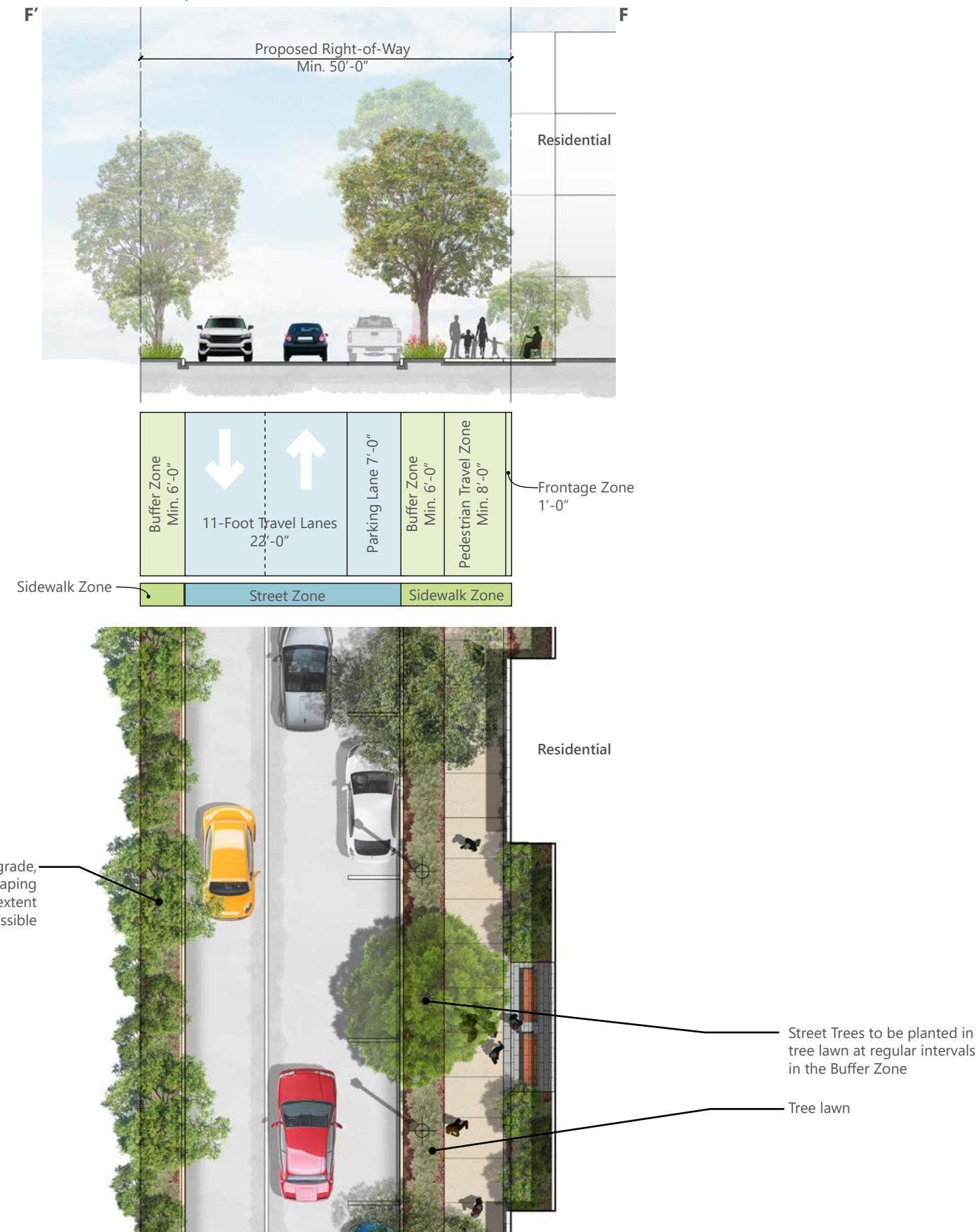
A service road along the south side of the Diamond District site will provide access to the baseball stadium for deliveries and emergency vehicles. The road will also provided limited access to some multi-family residential buildings as well as on street parking. Do to the change in grade along the south side of the site, the right-of-way may be 50'-0" wide. Tree lawns shall be used in the buffer zone rather than tree wells.

- Better Streets Manual Street Typology: Neighborhood Residential Streets
- Proposed Right-of-Way Width: 50'-0"

Service Road Approximate Location



Service Road Conceptual Section & Plan



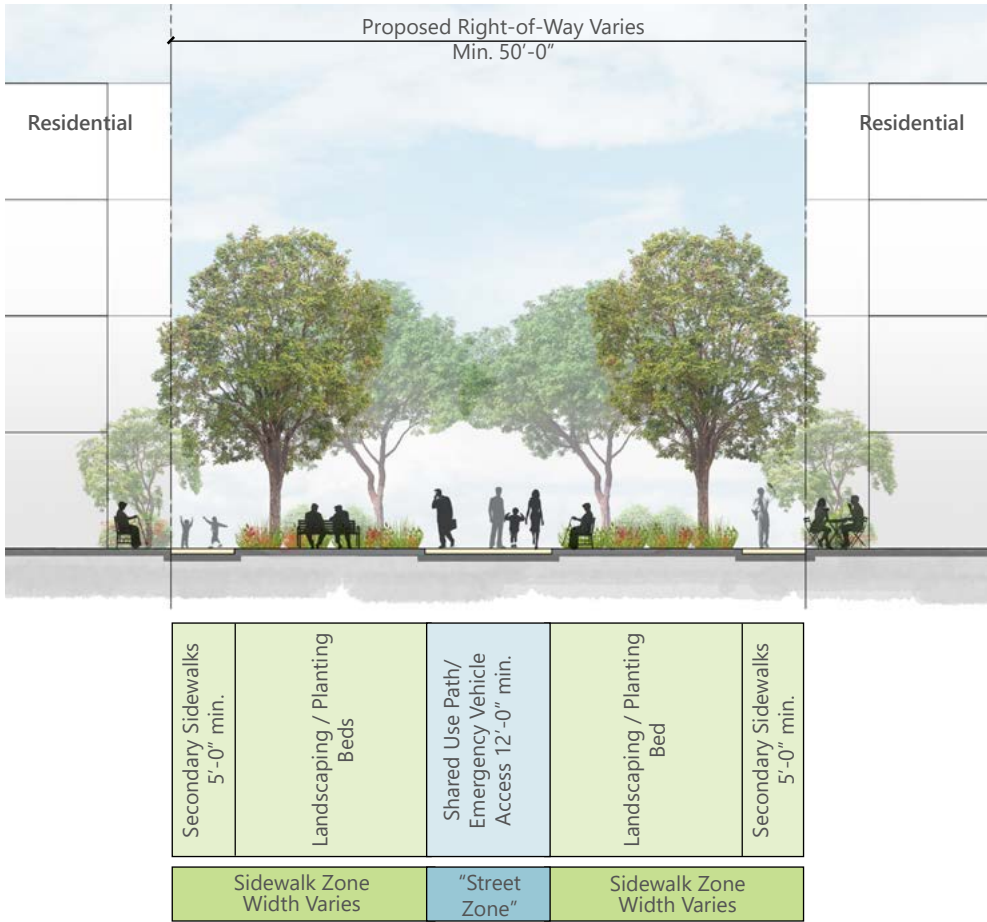
Mews Precedent Images



A public Mews that weaves between multi-family residential buildings shall provide a secondary north south pedestrian and bicycle route through the Diamond District in addition to the linear park. The mews is primarily intended to be used by residents of the adjacent buildings. A minimum 12' wide shared use path through the Mews shall be provided for pedestrian & bicycle circulation. This path shall be designed and constructed in manner that it can be used by emergency vehicles when necessary to access the residential units. Secondary sidewalks shall provided access to residential buildings. Landscaped areas with seating interspersed throughout the Mews shall be provided. Raised crosswalks shall be used where the public Mews intersects other streets. Refer to section 4.7 Public Gathering Space Considerations and section 5.3 Public Gathering Spaces for additional standards.

- Better Streets Manual Street Typology: None
- Proposed Right-of-Way Width: Varies, Minimum 50'-0"

Mews Conceptual Section & Plan



4 | PUBLIC REALM ELEMENTS



Credit: F11photo | Adobe Stock

4.1 CHAPTER INTRODUCTION

The purpose of this chapter is to provide a guide for addressing elements and situations that are common throughout the public realm and not specific to any one street or public space.

4.2 SUSTAINABILITY STRATEGIES

Incorporating sustainability strategies into the design of the public realm of the Diamond District will support human health and the health of the environment, improve the quality of life for all users, and help preserve this neighborhood for generations to come. Sustainability strategies aim to reduce some of the negative impacts that urban environments have on the natural environment by improving water, soil, and air quality, promoting biodiversity, treating stormwater in a manner that more closely mimics natural processes, reducing the consumption of materials and resources, and providing for alternative energy strategies. **Sustainability measures shall be incorporated into the design of the public realm to the greatest extent possible.** This includes but is not limited to the following:

- Selecting materials and products with low carbon and sustainable design elements
- Using regionally sourced materials and products
- Using recycled materials and products
- Selecting high durable, low maintenance materials and products.
- Providing electric vehicle charging stations for vehicles and e-bikes throughout the site
- Incorporating green infrastructure techniques to address stormwater
- Harvesting and reusing rainwater and greywater
- Installing pavement with high solar reflectance in sunny areas
- Increasing the urban canopy
- Using native plant species and removing invasive plant species
- Providing a community garden in the public realm
- Limiting the use of turf grass
- Using LED lighting and solar energy

Specific sustainability requirements can be found throughout this document. Refer to the Richmond Sustainable Design Standards for more information on the city’s sustainability initiative.



Credit: Think B | Adobe Stock

Electric Vehicle Charging Stations



Credit: Pure Energy Solar

Solar Bench



Credit: National Association of City Transportation Officials

E-bike Charging Stations

4.3 STORMWATER MANAGEMENT

The Diamond District developer shall create a comprehensive stormwater management plan for the Diamond District site for review and approval by the COR. This plan shall be implemented by the developer during design. The plan shall prioritize a centralized stormwater management system for the entire site under the linear park, supported with distributed green infrastructure facilities integrated into the streetscape. Larger green infrastructure practices, such as linear bioretention and permeable pavement shall be prioritized over smaller-scale green infrastructure to minimize maintenance needs. All green infrastructure shall consider pedestrian, bike, and vehicular safety. In addition to bioretention facilities and permeable pavement, green infrastructure may include vegetated swales, green/vegetated roofs, rainwater and greywater harvesting & reuse systems, infiltration basins, bioswales, tree box filters, and other practices approved by the City. If vegetated swales are used, Linear Park Perimeter Streets and Neighborhood Residential Streets are better suited for them as opposed to other street typologies. Where additional information is found during design that impacts the stormwater master plan, the developer shall notify the City of Richmond to discuss and agree to an alternative.



Credit: Berger Partnership

Bioretention Planting



Credit: City of Lake Oswego

Bioretention Planting



Credit: Oregon State University

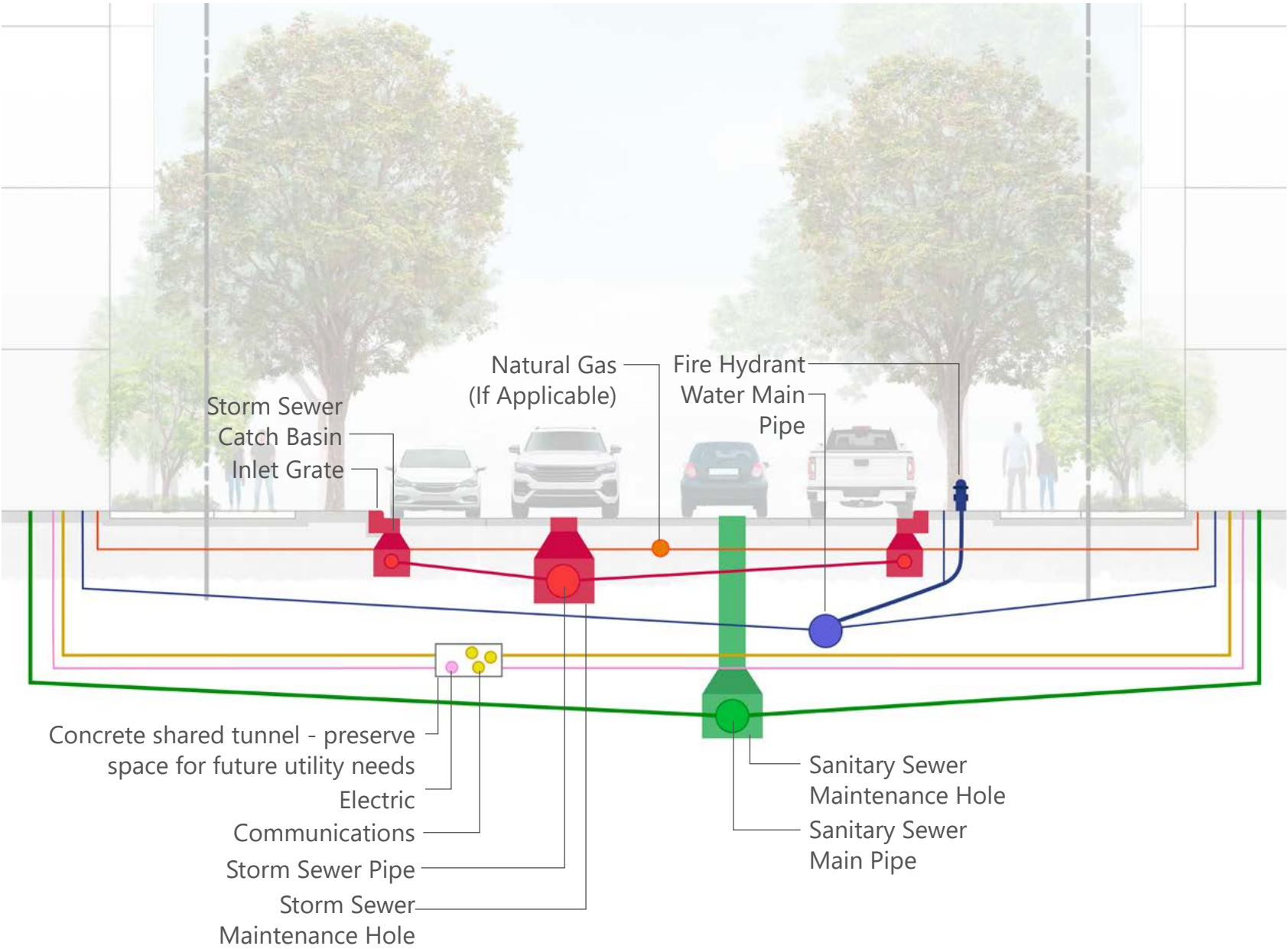
Vegetated Swale



Credit: Midwest City Oklahoma

Vegetated Swale

Conceptual Utility Section



Overhead power lines shall be located underground



Discrete utility access cover



Decorative utility access cover

The location of utilities must be carefully planned so that they can efficiently and effectively provide services while minimizing conflicts between the utility and other elements of the streetscape. Potential points of conflict between utilities and street trees must especially be avoided.

Design Standards

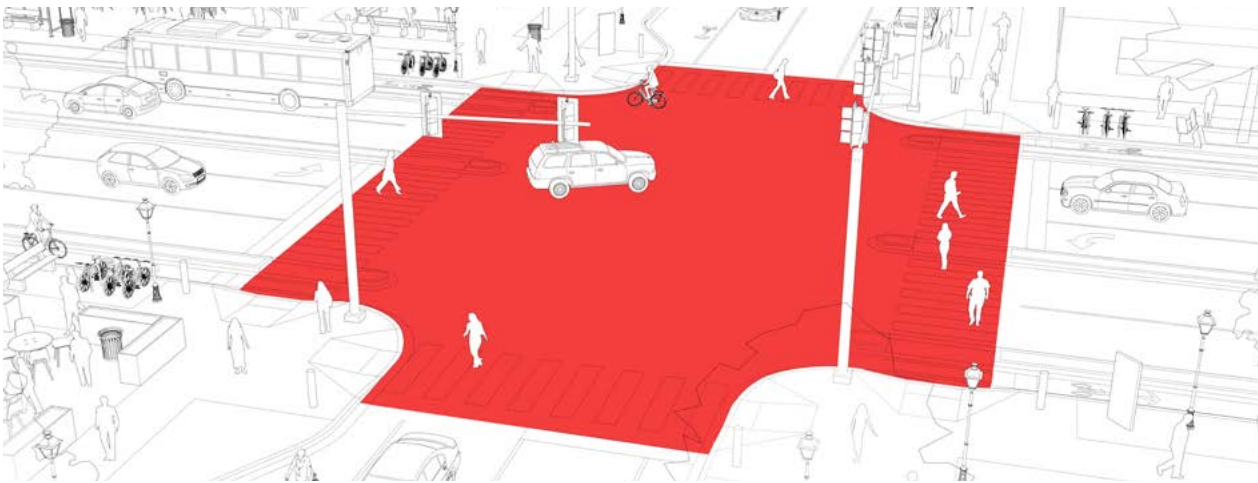
1. All utilities shall be located underground.
2. Utilities shall be situated within the roadway; should a utility need to be located within the sidewalk, a root barrier should be provided. Root barriers around laterals to buildings are recommended.
3. The City's Urban Design Guidelines recommend the development of a plan for a phased network of underground cable-ready infrastructure, which would consist of a series of interconnected hollow tubing which could accommodate existing and future wires and cables. Such a plan should be implemented in the Diamond District to minimize the impact of overhead wires and cables on the visual environment, facilitate the placement of existing overhead wires and cables underground, and accommodate future wires and cables in an appropriate manner.
4. All utility lines should be located so that they will not interfere with tree well locations.
5. Above ground gas meters and other development specific equipment must not be located within the right-of-way.
6. Trees shall be pruned or located in a manner to maintain a 6-foot clearance from any streetlight so that the tree doesn't damage the streetlight or interfere with light that is being cast.
7. Transformers and other at grade utility equipment shall be located as unobtrusively as possible, such as in vaults either under the sidewalk or in the Frontage Zone. They shall not be located in the Pedestrian Travel Zone or any pedestrian or bicycle path of travel.
8. Transformers shall be situated strategically to accommodate Level 3 EV charging.
9. Utility access covers and other access points to below-grade equipment located in the public realm shall be metal and finished in a color or pattern that matches or compliments the surrounding pavement.
10. Grates, utility covers, and similar shall all be ADA, baby stroller wheel, and heel appropriate.
11. Mechanical equipment that is visible from the street shall be screened using walls, landscaping, or other materials. See section 7.9 Screening and Fencing.
12. No tree plantings or shrubs greater than 5' in height at maturity are permitted within the existing 20' high pressure gas easement along Robin Hood Road. See section 3.5 Robin Hood Road.

4.5 INTERSECTIONS & CROSSWALKS

Intersections are an area where pedestrian, bike, and vehicular traffic converge. It is important that design allows for all users to safely move through the intersection.

Design Standards

1. Intersections should be designed to balance the needs of all road users, notably the most vulnerable users - pedestrians. Better Streets outlines three design principles for improving pedestrian safety and comfort: minimize crossing distance, minimize conflicts with turning vehicles, and provide sufficient signal time to cross the street at signalized intersections.
2. Curb extensions (bulb-outs) shall be used at all intersections where feasible. Curb extensions shall be long enough to ensure that no vehicle can park 20' from edge of crosswalk and 30' from stop sign.
3. Urban curb radii shall be used.
4. Refuge islands the width of the crosswalk shall be provided for streets with 4 or more travel lanes or over 60 feet of distance between curbs.
5. Crosswalks at signal controlled crossings and other crossing locations deemed appropriate by the City Traffic Engineer shall be high-visibility white block style pavement markings that avoid the predominant wheel tracking of the major through movements. Crosswalks at unsignalized crossings should be white transverse parallel lines. Crosswalks should be the same width as the sidewalk zone feeding them.
6. Crosswalks at unsignalized intersections on the Festival Street shall also be high-visibility white block style pavement markings due to the anticipated pedestrian volume. The use of stamped asphalt at these intersections may be used. Refer to section 3.6 Festival Street for more information.
7. Mid-block crosswalk shall only be permitted with the use of a raised crosswalk or a crosswalk with a pedestrian hybrid beacon. Raised crosswalks shall be used where the public Mews intersects other streets.
8. Raised crosswalks are permitted.
9. Pedestrian crosswalk signals shall be installed at all signalized intersections. Include a pedestrian delay on traffic signals.
10. "Two curb ramp crosswalks" where each curb ramp is directly aligned with a crosswalk shall be used at all intersections.
11. Trees at intersections shall be planted at least 25 feet away from the corner to allow for adequate line of sight in all directions. Landscaping placed within site triangles at intersections must not exceed 12 inches in height.
12. Bicycle lanes through intersections must be highly visible through the use of pavement markings and signage.
13. The use of roundabouts may be considered at certain intersections in order to more safely and effectively move vehicular, pedestrian, and bicycle traffic.
14. **A 10' wide pedestrian path of travel from the main game day parking garage(s) to the ballpark must be provided.**



White Block Style Crosswalk



Stamped Asphalt



Raised Crosswalk

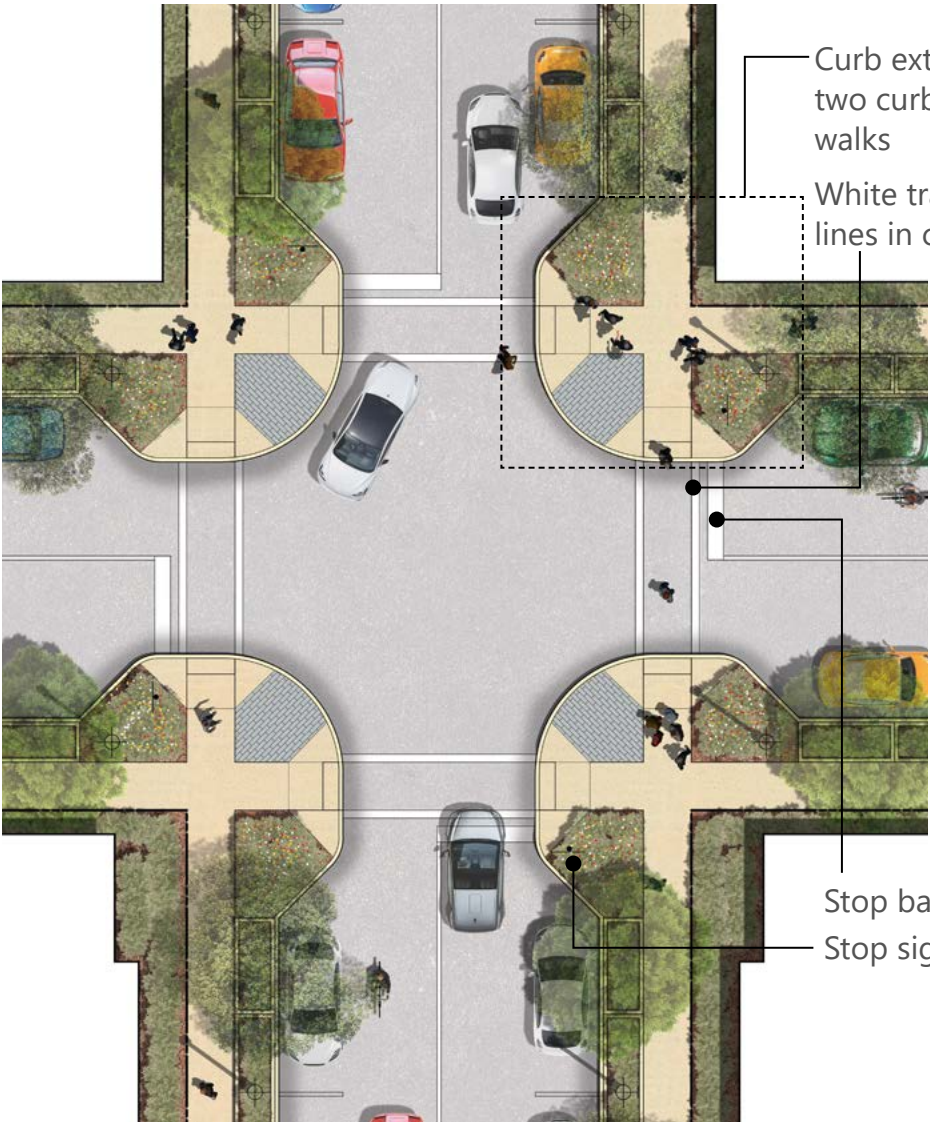


Curb Extension



Roundabout

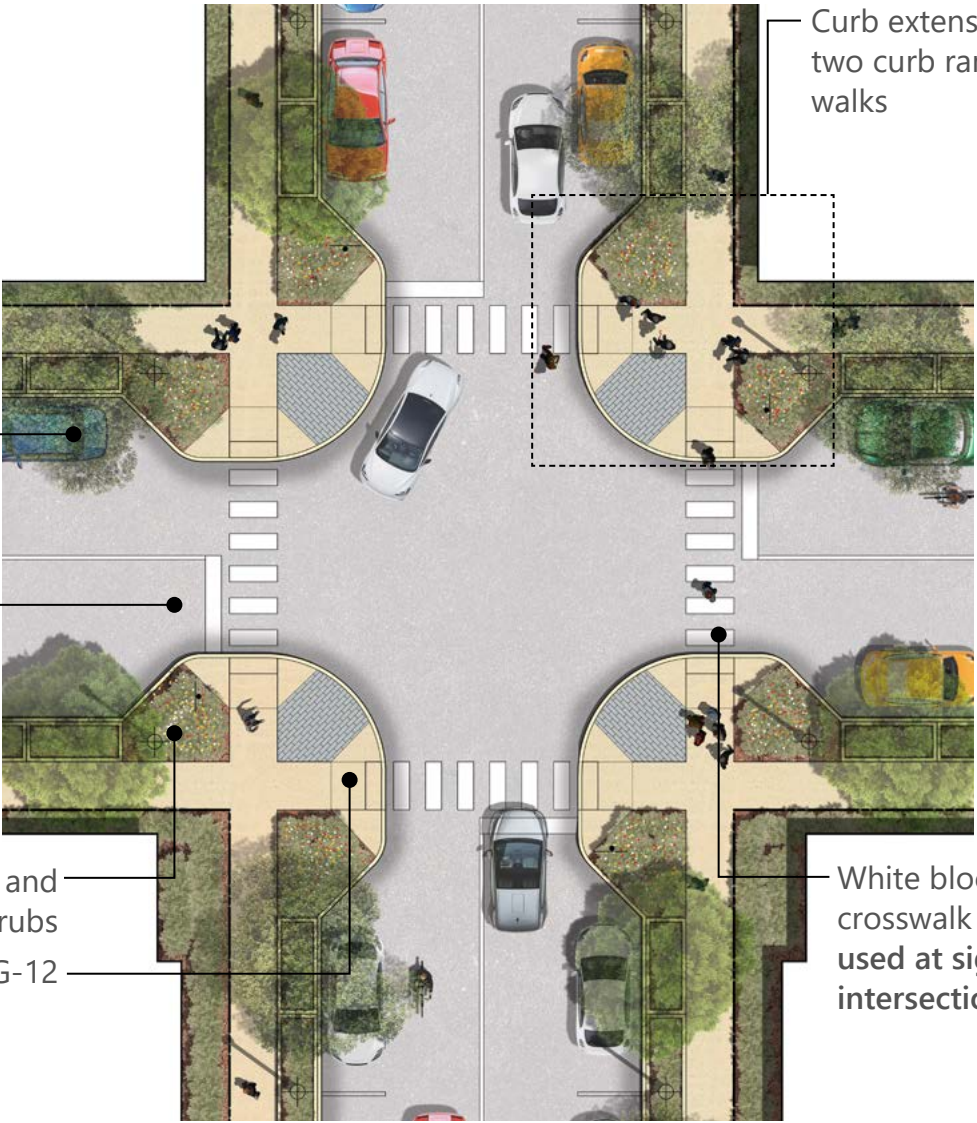
Conceptual Intersection Plan -
Unsignalized Intersection with Curb Extensions



Parking lane
Vehicle travel lane

Low ground cover and shrubs
CG-12

Conceptual Intersection Plan -
Festival Street Unsignalized Intersections



Curb extension with two curb ramp crosswalks

White block style crosswalk (also used at signalized intersections)

ADA ramps do not need wing walls when adjacent to a non-traversable surface, such as grass or mulch



4.6 CURBSIDE MANAGEMENT

Activity along the street generated by adjacent land uses places demands on the curb that require effective curbside management strategies. Limited curb space is increasingly in demand by competing factions. The growing list of curbside activities in the Diamond District may include:

- Long term and short term on-street parking for personal vehicles.
- Parking for emergency vehicles and police
- Passenger pick-up and drop off
- Parking for parcel and food delivery
- Parking for commercial delivery
- Transit stops
- Parking spaces with electric vehicle charging stations
- Parking for vendors and food trucks
- Stormwater management
- Curb cuts for driveways and alleys
- Parklets/curbside dining



Credit: Teltonika
EV Charging Station



Credit: Parking Lot Striping Atlanta
Fire Lane Markings



Credit: Soapbox Cincinnati
Food Trucks



Credit: Tryba Architects



Credit: Venture Richmond



Credit: Evanston Now
Curbside Dining

Design Standards

A curbside management plan that corresponds to activities associated with adjacent land uses shall be developed for each block within the Diamond District. A wide variety of curbside management strategies that may be employed include:

- Using the same space for multiple designated curbside activities such as combining passenger pick up and drop off with parcel and food delivery zones, all of which are short term uses.
- Allocating permitted curbside uses by the time of day based on the needs of surrounding land uses.
- Dividing blocks into different zones with each zone accommodating a different curbside activity.
- Placing time limits or fees on certain activities at peak use times.
- Assigning specific spaces for specific activities or restricting the type of activities that are permitted.
- Implementing a curbside reservation system.
- Designating certain curb space for parking by permit only.

The following design standards apply to the Diamond District:

1. Curbside regulations, including permitted and prohibited activities, shall be clearly communicated to users via signage and proper street markings.
2. Emergency vehicle parking shall be located adjacent to the ballpark and shall be marked with signage and special asphalt paint pattern.
3. Fire Lanes shall be marked with signage and red indicator paint on the curb for the extent of the fire lane. Fire Lanes shall be pre-planned so that street parking can be maximized.
4. A high turnover/loading on-street parking spot shall be provided at the end of every block on both sides of the street when street parking is provided. High turnover locations shall be marked with signage and a special asphalt paint pattern. High turnover locations shall be signed as 15-minute parking or rideshare/delivery parking.
5. On-street parking shall be provided on both sides of the street as often as possible.
6. Parking spaces with electric vehicles supply equipment shall be provided throughout the Diamond District. A mix of Level 2 and Level 3 chargers shall be provided throughout the site.
7. Curbside space along the Festival Street shall be designed to accommodate vendors and food trucks during special events. Electrical hook-ups shall be provided at these locations.
8. A school bus drop off area shall be located on the Festival Street or adjacent to the Stadium Plaza.

4.7 PUBLIC GATHERING SPACE CONSIDERATIONS

Well planned public gathering spaces can encourage activity and draw people to the Diamond District year-round. Spaces should offer a wide range of activities and meet a wide range of users needs. Careful consideration shall be given to how each space will be used. The landscape palette, hardscape materials, and site furnishings are the elements that will tie each space together and make it recognizable as being part of the Diamond District. Public gathering spaces include spaces within the right-of-way, the linear park, the stadium plaza, the public Mews, and other areas as designated.

General Design Standards

1. Gathering spaces shall be visually and physically connected to the street network via a network of tree lined sidewalks, shared use paths, and other pedestrian and bicycle circulation routes.
2. A variety of gathering spaces shall be incorporated into the public realm; spaces for active and passive recreation, large and small groups, and people of various ages and abilities. Programmed activity areas as well as flexible use spaces that can be adapted for a variety of uses and activities shall be provided.
3. Public gathering spaces must be compatible with adjacent land uses.
4. Gathering spaces shall contain the appropriate levels of seating, lighting, shade, plantings, and other amenities to make them desirable places in which to spend time.
5. Areas of shade shall be provided throughout public gathering spaces, especially at seating areas. Landscaping or structures may be used to provide shade.
6. Pedestrian scale lighting shall be provided throughout gathering spaces.
7. Large gathering spaces shall have numerous entrances and exits.
8. The pedestrian path of travel shall be a minimum of 8’ wide and clearly delineated within a gathering space.
9. Gathering spaces shall be buffered from Street Zone through the use of street trees, landscaping, planters, and bollards. See section 7.8 Bollards & Planters for more information.



Credit: Federal Realty Investment Trust

Special treatment at street corner



Credit: WRT Design

Gathering space in the buffer zone



Credit: West Orange

10. Incorporate existing natural features and utilize sustainable design practices to the extent possible.
11. Seating and spaces for vendor kiosks shall be incorporated into the Buffer and Frontage Zones at key locations through the Diamond Site.
12. At least one community garden shall be located within the Diamond District near residential areas.
- 13. A 10’ wide pedestrian path of travel from the main game day parking garage(s) to the ballpark must be provided.**

Linear Park Design Standards

In additional to the general design standards, the following standards shall apply to the Linear Park:

1. A community garden shall be provided.
2. Restrooms shall be provided within the linear park.
3. A 12’ pedestrian path that is centrally located and runs east to west through the park shall be provided. This path shall be designed and constructed to accommodate emergency vehicles in order to access or transverse the park in the event of an emergency. It may also be use as an access point for concert service vehicles, maintenance vehicles, and food trucks within the park.
4. Per the Diamond District development agreement, the Arthur Ashe Jr. Legacy Path shall weave through Diamond District within the linear park from north to south.

Stadium Plaza Design Standards

A large pedestrian plaza shall be provided in front of the baseball stadium’s main entrance and ticket area. In additional to the general design standards, the following standards shall apply to the plaza:

1. A large bike parking location that provides bike racks to accommodate 200 bikes shall be provided adjacent to the stadium for stadium attendees. To avoid pedestrian/cycling conflicts, bike parking shall not impede accesses to the main gates, but shall be located within a short walking distance. Bike parking may be covered.
2. Vehicular access to the stadium plaza shall be limited to emergency vehicles and designated food truck areas.
3. Shaded seating areas shall be provided within the plaza for gathering and meeting prior to a game.
4. A diamond scoring pattern may be used for pedestrian circulation routes within the stadium plaza and shall be coordinated with the paving pattern on the Festival Street.
5. Special landscaping, such as a tree lined path, shall be used to emphasize the entrance to the stadium.
6. A location for public art shall be designated within the stadium plaza. See section 7.10 Public Art.
7. A school bus drop off area shall be located adjacent to the Stadium Plaza or on the Festival Street.

4.8 PUBLIC TRANSIT CONSIDERATIONS

Design Standards

- 1. New or relocated transit stops shall be determined via a collaborative effort between the Greater Richmond Transit Company (GRTC) and the City’s Department of Public Works (DPW) and Department of Planning and Development Review (PDR).
- 2. Transit stops shall not be located mid-block unless adjacent to a crosswalk with a pedestrian hybrid beacon or raised crosswalk.
- 3. Transit stops shall be located within curb extensions for any stops on the interior of the Diamond District site. Curb extensions shall not be used along N. Arthur Ashe Boulevard, Hermitage Road, or Robin Hood Road.
- 4. Areas around transit stops should be well lit to provide greater visibility and safety at night.
- 5. See section 7.11 Essential Transit Infrastructure for information on transit shelters and other transit infrastructure.



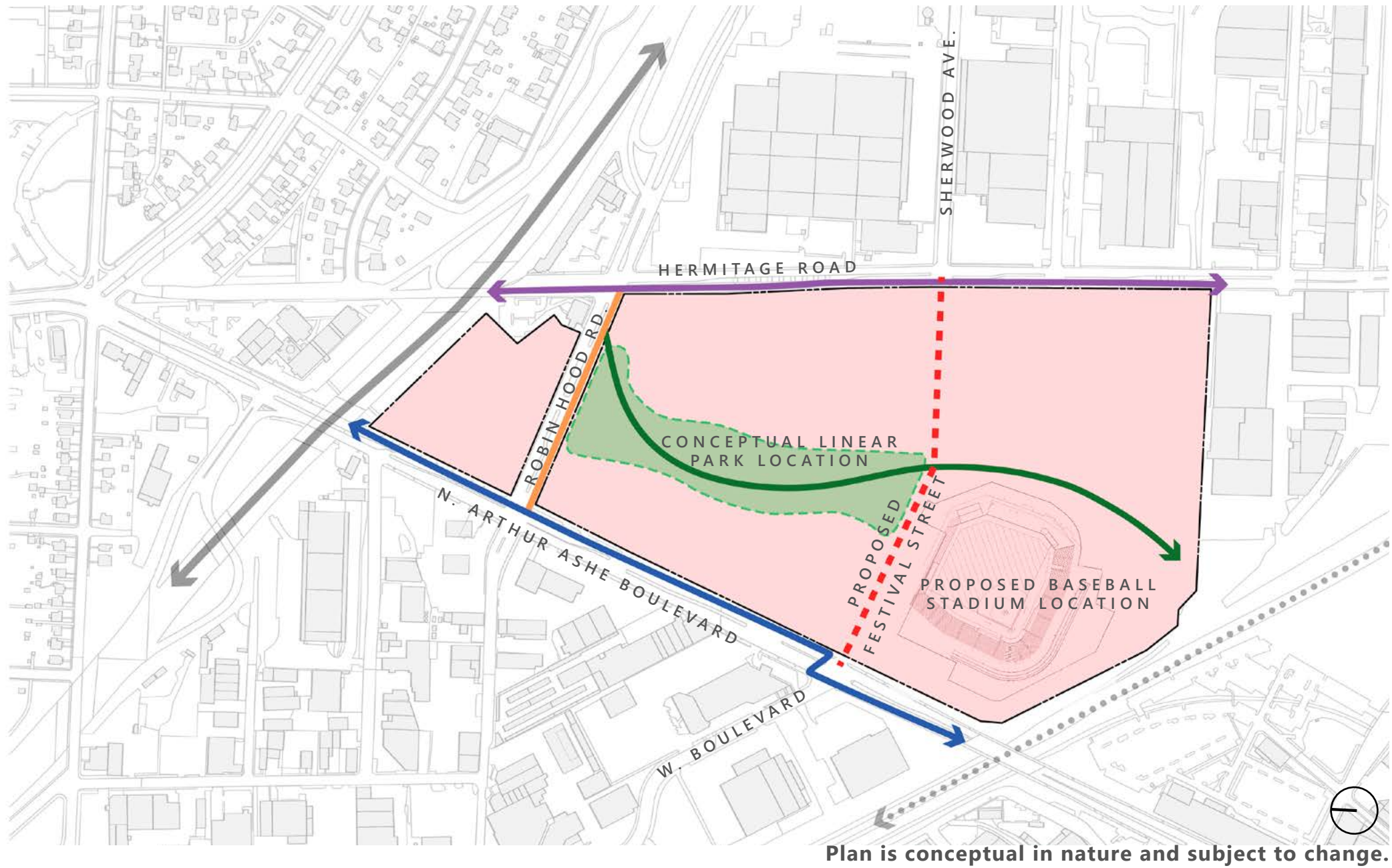
Credit: Mass Transit Mag
Curb Extension At Transit Stop



Credit: Transportation for America
Pulse Transit Stop

4.9 BICYCLE CONSIDERATIONS

Conceptual Bicycle Circulation Routes



Bicycle facilities within the Diamond District that connect to the greater city wide bicycle network should be provided along with associated bicycle amenities.

Design Standards

1. The Fall Line Trail shall be located on the west side of Hermitage Road. It shall be a shared use path.
2. Buffered bike lanes shall be provided along N. Arthur Ashe Boulevard.
3. Per the Diamond District development agreement, the Arthur Ashe Jr. Legacy Path shall weave through the linear park from north to south. The path shall be a shared use path.
4. Bicycle lanes, including direction of travel, must be clearly marked with pavement markings and signage. Bicycle lanes through intersections must be made highly visible through the use of pavement marking and signage.
5. Bike crossings shall be signalized.
6. See section 03 Street Network & Streetscape Standards information regarding N. Arthur Ashe Boulevard, Hermitage Road, and Robin Hood Road.
7. See section 7.6 Micromobility Amenities regarding bike storage and other bike related amenities.

- Existing I-95 / I-64
- Fall Line Trail
- Proposed Buffered Bike Lanes
- Arthur Ashe Legacy Path
- Shared Use Path
- Proposed Festival Street
- Existing Railroad

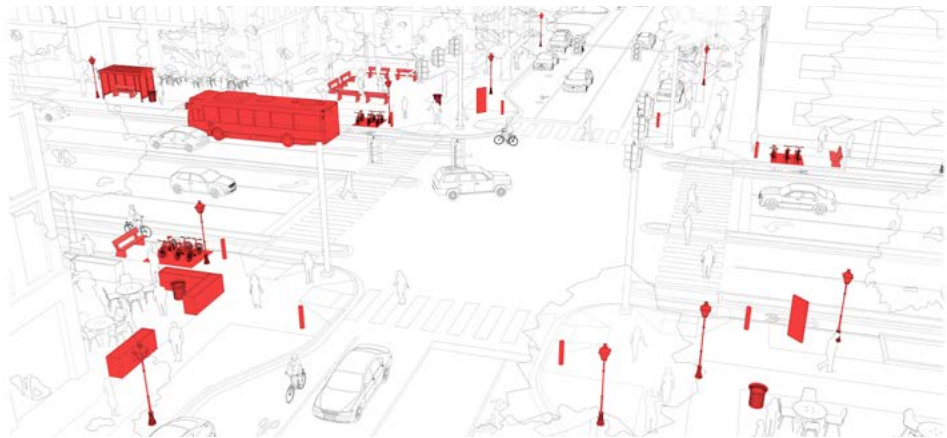
4.10 BRANDING CONSIDERATIONS

The Diamond District will be a special neighborhood destination within the City of Richmond. A recognizable branding campaign linked to a greater strategy of marketing the development shall be undertaken. A recognizable branding theme shall celebrate and promote the aspects of the Diamond District that make it a unique place within the city - particularly the ballpark and the linear park. Successful branding can contribute to making the Diamond District a vibrant and economically successful community by:

- Generating excitement about the Diamond District, thus drawing the attention of potential residents, businesses, and visitors to the District
- Encouraging a sense of belonging and community pride
- Encouraging community collaboration efforts

Design Standards

1. A style guide that includes acceptable fonts and colors shall be developed that communicates the Diamond District vision as a vibrant, inclusive mixed-use community.
1. A logo and slogan for the Diamond District shall be developed that can be consistently applied to elements throughout the public realm.
2. The Diamond District logo shall be applied to banners, benches, wayfinding, and interpretive signage.
3. Gateway features shall be used to market the District and define its edges.
4. Banners shall be placed on all light poles along streets to promote the Diamond District as a whole and to promote special events and festivals within the District.
5. Sidewalk medallions that promote the Diamond District at key locations may be installed.
6. Artwork or murals that celebrate the Diamond District may be installed.
7. See section 7 - Site Furnishings and Amenities for more information on signage, wayfinding, gateways, banners, and public art.



Examples of Community Branding



4.11 SMART CITY ELEMENTS

Examples of Smart City Elements



Credit: Cities Today



The City of Richmond envisions a future where digital solutions integrate seamlessly into operations, infrastructure, and services, optimizing efficiency, accessibility, and sustainability. By prioritizing community engagement, collaboration, and urban planning, we create a resilient, vibrant city.

The Diamond District is envisioned to be developed as a Smart City innovation hub, which will further advance the City's goals and objectives related to:

- 1) Enhanced Public Engagement and Accessibility
- 2) Efficient Government Operations and Resources Management
- 3) Economic Development and Innovation
- 4) Infrastructure Modernization and Resilience
- 5) Environmental Sustainability and Quality of Life.

As the City further develops its vision and role as a Smart City, the Diamond District development will account for new and emerging technology solutions to meet these goals. Smart City elements shall be prioritized to the extend possible in order to create the City's first-ever Smart District. Immediate technology solutions to be incorporated into the District are:

- Public Wi-Fi
- Charging stations for electronic devices, which may be located in seating throughout the community
- Smart adaptive LED lighting
- Environmental sensors
- Smart parking for daily use and event use
- API to link together various smart technology solutions
- Street cleaning, landscaping, and maintenance services
- Traffic management
- Wayfinding/messaging

5 | PAVING & SURFACE MATERIALS



Credit: F11Photo | Adobe Stock

5.1 CHAPTER INTRODUCTION

Paving materials and patterns in the Diamond District should be used to define spaces within the public realm, add beauty and texture, draw attention to special features, and provide visual cues to help pedestrian, bicycle, and vehicular traffic safely navigate the Diamond District. It is important that materials and patterns are well coordinated throughout for a cohesive, comfortable environment. The use of too many paving materials or patterns can be distracting or unsettling, detracting from the outdoor spaces by competing with other elements of the public realm. Too few materials and patterns, and the hardscape becomes a bland background that doesn't serve the aforementioned purposes. Paving should serve as a neutral surface with simple patterns and clean lines to help highlight and enhance other aspects of the public realm including the landscape, amenities, and site furnishings. Paving selections may vary from street to street provided that there is a seamless transition between streets. Paving in the public realm shall be selected based on durability and performance, maintenance requirements, cost, aesthetics, compatibility with adjacent materials and landscaping, and environmental considerations.

5.2 THE SIDEWALK ZONE

The Sidewalk Zone consists of the Frontage Zone, the Pedestrian Travel Zone, and the Buffer Zone. Each Zone serves a different purpose. The Frontage Zone serves as a buffer between pedestrians in the Pedestrian Travel Zone of the sidewalk and activities associated with adjacent buildings, including opening doors, storefront seating, and outdoor dining. The Pedestrian Travel Zone is reserved for active pedestrian use and must be free of all impediments. The Buffer Zone provides a safe separation between pedestrians in the Pedestrian Zone and vehicles in the Street Zone. Paving shall be used to differentiate each zone of the sidewalk.

General Design Standards

- 1. Poured concrete shall be natural in color. Dyed or stained concrete is not permitted.
- 2. Stamped concrete is not permitted.
- 3. All joints in poured concrete shall have a tooled edge. Decorative joints in poured concrete shall be hand tooled.
- 4. Poured concrete shall have a broom finish perpendicular to the direction of travel.
- 5. Concrete pavers shall be natural color or gray tones.
- 6. Pavers shall be square edge pavers with hand tight joints.
- 7. Paving shall have a slip resistant surface.
- 8. Changes in materials and scoring or paving patterns shall be used to differentiate the Frontage Zone, the Pedestrian Travel Zone, and the Buffer Zone. Where more than one mode of mobility exists in the Pedestrian Travel Zone (ie pedestrians and bikes), scoring shall be used to differentiate between modes. Materials and patterns shall be complimentary to each other to create a cohesive environment.



Scoring pattern differentiates the Buffer and Pedestrian Travel Zones



Credit: Realokie Concrete

Diamond scoring pattern



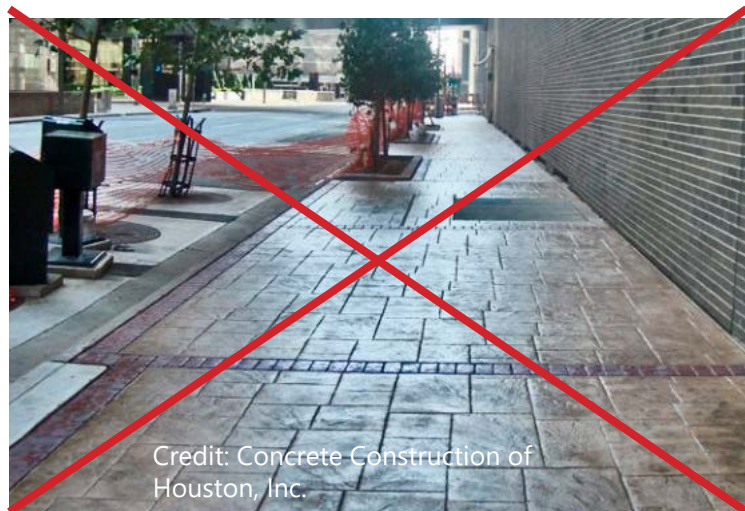
Credit: Annandale Today

Materials and paving patterns differentiate the Buffer and Pedestrian Travel Zones



Credit: Metten Design

Plank style pavers



Credit: Concrete Construction of Houston, Inc.

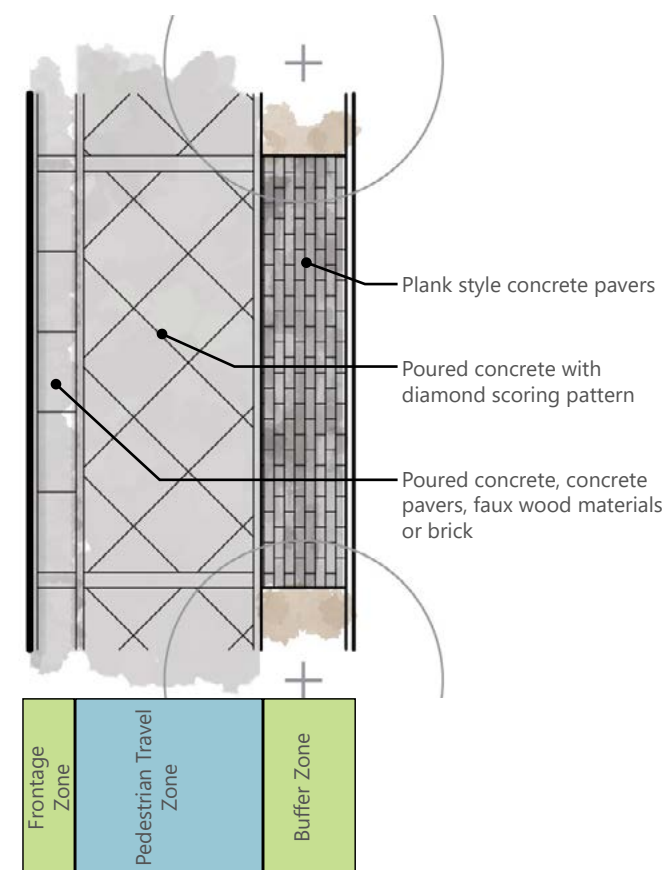
Dyed and/or stamped concrete is not permitted.



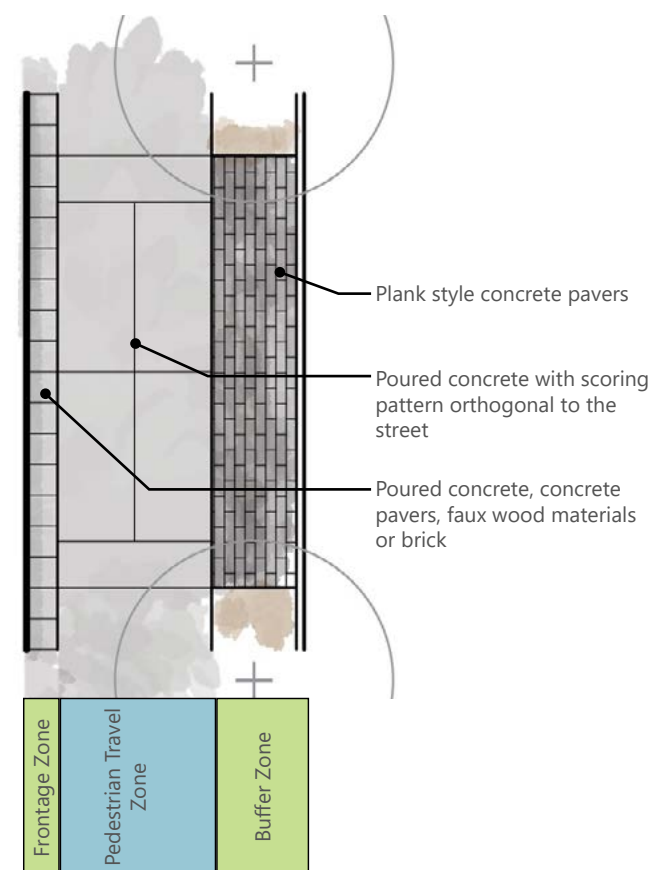
Credit: Tenax USA

There is no differentiation between sidewalk zones.

*Conceptual Paving Pattern -
Festival Street*



*Conceptual Paving Pattern -
Typical Street*



Frontage Zone Design Standards

1. Poured concrete, concrete pavers, faux wood materials or brick may be used in the Frontage Zone. Frontage zone materials shall not be pervious to ensure structural integrity of building foundations.
2. Paving materials and patterns in the Frontage Zone shall be coordinated with the hardscape within the setback to create a seamless transition.

Pedestrian Travel Zone Design Standards

1. Paving in the Pedestrian Travel Zone shall be poured concrete with a consistent scoring pattern no tighter than 3' x 3'. An occasional strip of concrete or concrete pavers with a varied scoring pattern tighter than 3' x 3' may bisect the Pedestrian Travel Zone to emphasis a significant feature, such as an important buildings entrance or open space.
2. Scoring or paving patterns shall be looser in the Pedestrian Travel Zone than in the Frontage or Buffer Zones so as to clearly delineate the pedestrian realm.
3. A diamond scoring pattern shall be used in Pedestrian Travel Zone of the Festival Street. A scoring pattern that is orthogonal to the street shall be used on all other streets.
4. Paving shall continue across driveway aprons and service entry drives that cross the Pedestrian Travel Zone to maintain a consistent streetscape material for the length of the sidewalk. A distinguishing band of material or change in scoring pattern shall clearly highlight the edge of the drive, visually marking the transition from the sidewalk to the driveway crossing.

Buffer Zone Design Standards

1. Paving materials in the Buffer Zone shall be poured concrete or concrete pavers.
2. Plank style pavers shall be used.
3. The use of permeable paving systems shall be used in used in the Buffer Zone as often as possible. Selected permeable pavers must be able to receive winter treatment.
4. Permeable pavers shall be spaced as far apart as possible while still conforming to ADA requirements.
5. Balance paving selection and construction with the installation needs of street trees and landscaping to allow for trees to flourish and hardscape to remain in good condition.
6. Special paving treatment at significant street corners, such as a special paving pattern, is encouraged.

5.3 PUBLIC GATHERING SPACES

Paving can be used to define these spaces, much as it is used to differentiate the three different Sidewalk Zones. Public gathering spaces shall include spaces within the sidewalk zone of right-of-way, the linear park, the stadium plaza, the public mews, and other areas as designated.

Design Standards

1. The use of permeable paving systems shall be used in public gathering spaces as often as possible. Pavers shall be spaced as far apart as possible while still conforming to ADA requirements.
2. Pedestrian circulation routes must be at least 8’ wide through public gathering spaces, unless otherwise designated. They shall be poured concrete or concrete pavers with a consistent scoring pattern no tighter than 3’x3’. An occasional strip of concrete or concrete pavers with a varied scoring pattern tighter than 3’ x 3’ may bisect the pedestrian circulation routes to emphasis a significant feature, such as a building entrance or a fountain or to differentiate the gathering space where it meets the Sidewalk Zone.
3. Paving materials used outside of pedestrian circulation routes shall be poured concrete or concrete pavers.
4. Poured concrete shall be natural in color. Dyed or stained poured concrete is not permitted.
5. Stamped concrete is not permitted.
6. Poured concrete used in pedestrian circulation routes should have a broom finish perpendicular to the direction of travel.
7. All joints in poured concrete shall have a tooled edge. Decorative joints in poured concrete should be hand tooled.
8. Concrete pavers shall be natural color or gray tones.
9. Paving patterns should be well thought out and designed to draw attention to significant features, such as an important buildings entrance, public art, or a fountain. Variations in hardscape along with landscaping should be used to differentiate “rooms” within a gathering space or outdoor seating areas. Paving materials and patterns shall be complimentary to each other to create a cohesive environment. Landscaping should be used to break up and soften large expanses of paving in public gathering spaces.
10. Paving shall have a slip resistant surface.
11. Selected permeable pavers must be able to receive winter treatment.
12. Pavers shall be square edge pavers with hand tight joints. Permeable pavers shall be spaced as far apart as possible while still conforming to ADA requirements.
13. Balance paving selection and construction with the installation needs of trees and other landscaping to allow for trees to flourish and hardscape to remain in good condition.



5.4 BUFFERED BIKE LANES & SHARED USE PATHS

Design Standards

1. Bike lanes and shared use paths located within the Sidewalk Zone shall be poured concrete. Asphalt is not permitted.
2. Permeable pavers shall be used for bike paths and shared use paths that are located in an independent right-of-way or easement separate from the Sidewalk Zone. Additional surface materials may be used with approval if wetlands or fragile eco-systems are present.
3. Bike lanes and shared use paths shall be clearly marked with lane stripping and arrows to show the direction of travel and to differentiate between pedestrian and bike circulation.
4. Crosswalks where bike lanes or shared use paths intersect with vehicular traffic shall be properly marked.
5. The use of porous asphalt is not permitted.

5.5 THE STREET ZONE

The Street Zone is located within the right-of-way between the two opposite curbs. The Street Zone consists of the Parking Lane and the Travel Lane. The Street Zone is for vehicle and bicycle travel, parking, and other curbside activities.

Design Standards

1. Vehicular Travel Lanes shall be asphalt.
2. A permeable concrete paving system may be used with City approval for Parking Lanes in the Street Zone and in alleys. Selected permeable pavers must be able to receive winter treatment. Concrete pavers shall be natural color or gray tones.
3. Pavers shall be square edge pavers with hand tight joints. Permeable pavers shall be spaced as far apart as possible while still conforming to ADA requirements.
4. Bike lanes located in the Street Zone shall be asphalt. Bike lanes should be properly marked with directional signage and to differentiate them from vehicular travel lanes. Asphalt paving may be painted solid green or with green stripes to draw attention to bike lanes at areas where bike and vehicular traffic may conflict such as intersections.
5. The use of porous asphalt is not permitted.
6. Grass block or turf block pavers are not permitted.
7. See section 4.5 Intersections & Crosswalks for information regarding materials in crosswalks.
8. See section 3.6 Festival Street for information regarding the treatment of the Street Zone in the Festival Street.
9. Any coatings applied to asphalt, such as lane markings and street murals, shall have a reflective coating.



Poured concrete buffered bike lane



Poured concrete bike path



Permeable pavers in parking lane



Permeable pavers in parking lane



Porous asphalt is not permitted